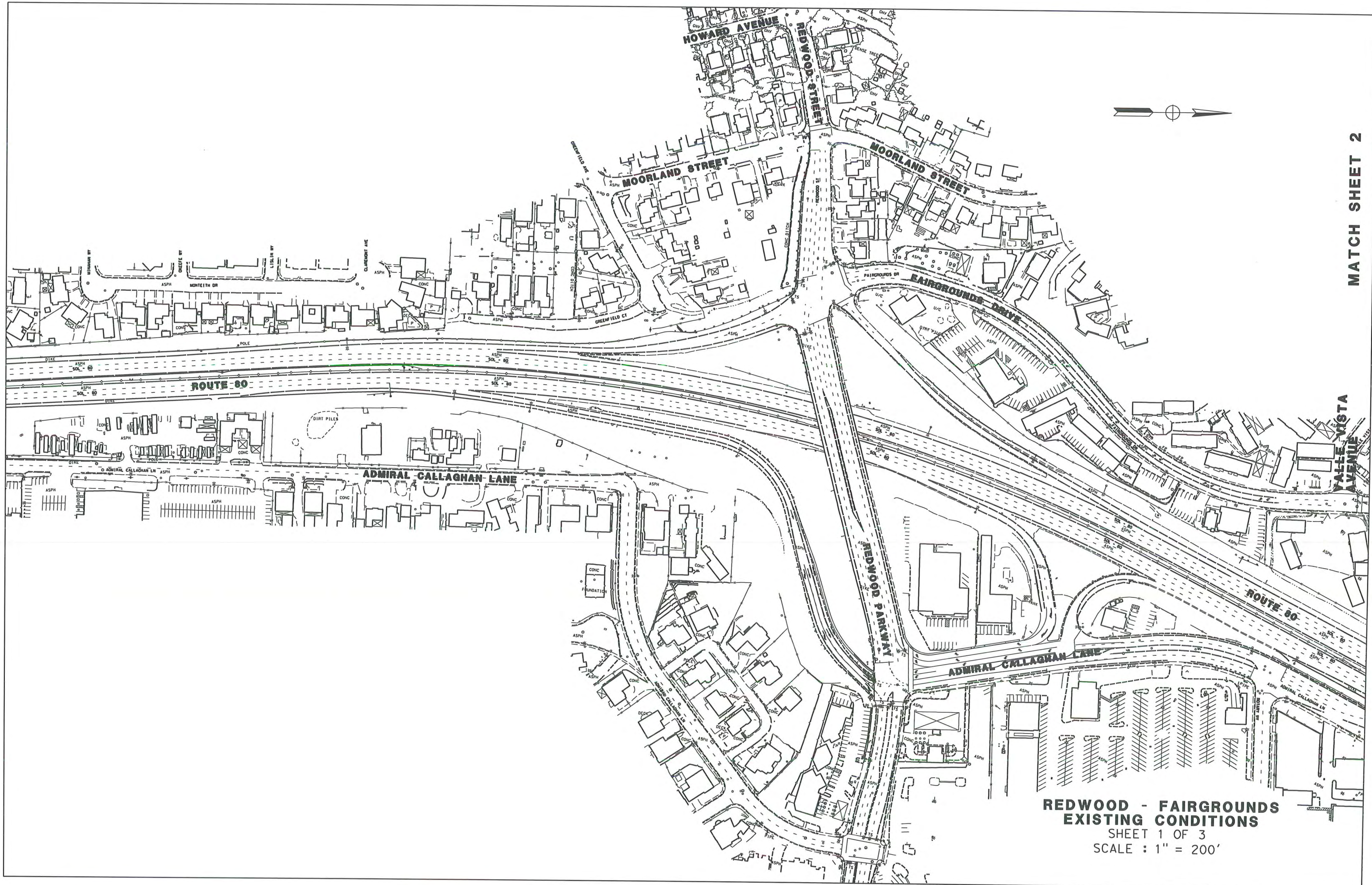
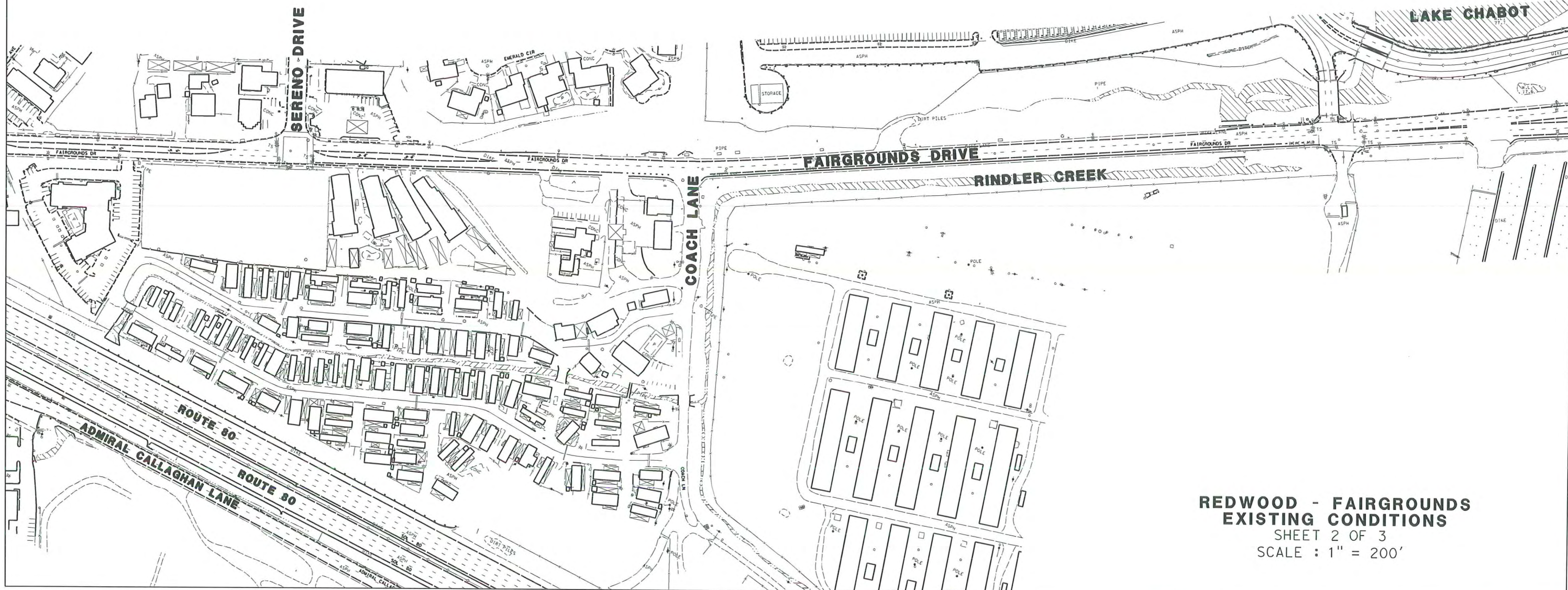


Attachment A

Existing Conditions

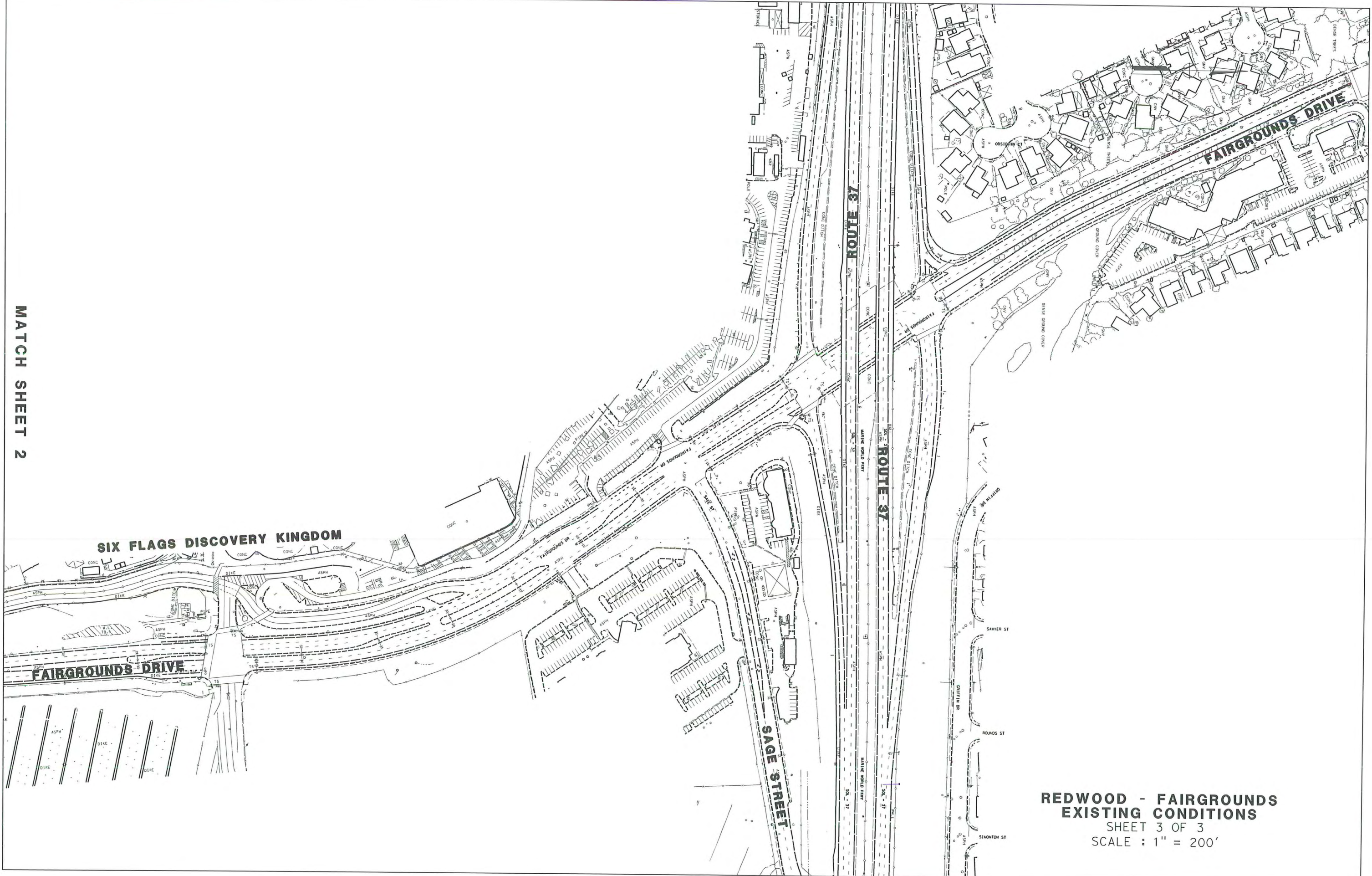


**REDWOOD - FAIRGROUNDS
EXISTING CONDITIONS**
SHEET 1 OF 3
SCALE : 1" = 200'



REDWOOD - FAIRGROUNDS
EXISTING CONDITIONS
SHEET 2 OF 3
SCALE : 1" = 200'

MATCH SHEET 2



REDWOOD - FAIRGROUNDS
EXISTING CONDITIONS
SHEET 3 OF 3
SCALE : 1" = 200'

Attachment B
Typical Cross Sections, Layouts and
Profiles
Build Alternative

NOTES:

1. SUPERELEVATIONS ARE SHOWN ON THE SUPERELEVATION DIAGRAMS UNLESS OTHERWISE NOTED.
2. FOR LOCATIONS, TYPE, AND LIMITS OF CONCRETE BARRIERS, RETAINING WALLS, AND CURBS, SEE LAYOUT SHEETS.
3. STRUCTURAL SECTIONS SHOWN ARE FOR PLANNING PURPOSES ONLY. FINAL SECTIONS TO BE DETERMINED DURING PS&E PHASE.

ABBREVIATIONS:

- BSWBACK OF SIDEWALK
- RAC-GRUBBERIZED ASPHALT CONCRETE (TYPE G)

DistCOUNTYLOCATION CODEPOST MILESTOTAL PROJECTSHEET No.TOTAL SHEETS

04Sol37, 804.0/4.910.6/11.2

REGISTERED CIVIL ENGINEER DATE

PLANS APPROVAL DATE

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Solano Transportation AuthorityOne Harbor Center, Suite 130Suisun City, CA 94585

REGISTERED PROFESSIONAL ENGINEER

No.

Exp.

CIVIL

STATE OF CALIFORNIA

STRUCTURAL SECTION NOTES:

- 1

0.15' RAC-G
0.23' AC (TYPE A)
0.50' CL2 AB
0.83' CL4 AS
- 2

0.15' RAC-G
0.33' CL2 AB
1.21' CL4 AS
- 3

0.15' RAC-G
0.35' AC (TYPE B)
0.20' ATPB
0.55' CL2 AB
0.95' CL4 AS
- 4

0.45' AC (TYPE B)
0.25' ATPB
0.60' CL2 AB
1.20' CL1 PERM MTL (BLANKET)
- 5

0.30' AC (TYPE B)
0.55' CL2 AB
1.65' CL1 PERM MTL (BLANKET)
- 6

0.40' AC (TYPE B)
0.60' CL2 AB
1.00' CL1 PERM MTL (BLANKET)
- 7

0.10 RAC-G
0.25' AC (TYPE A)
0.65' CL2 AB
1.05' CL4 AS
- 8

0.20' AC (TYPE A)
0.50' CL2 AB
1.35' CL4 AS
- 9

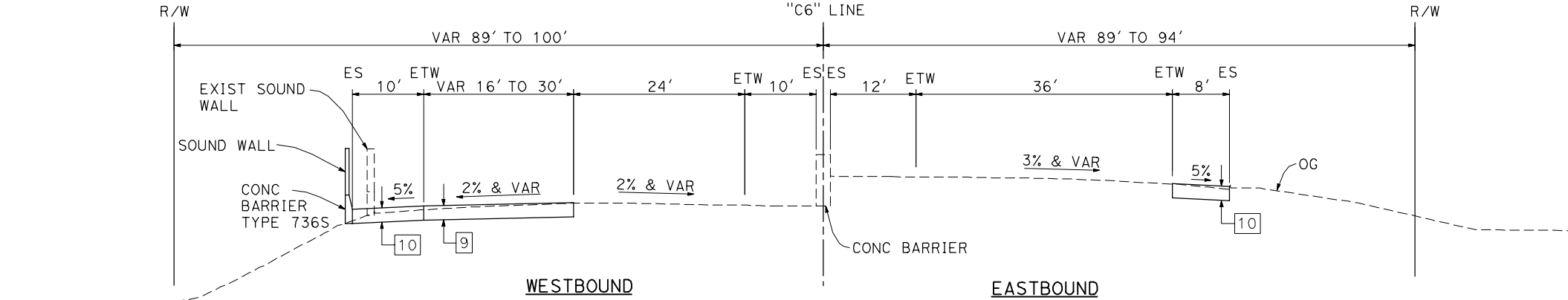
0.16' OGAC
0.15 RAC-G
0.18' AC (TYPE A)
0.55' PCC
- 10

0.10' OGAC
0.15' RAC-G
0.18' AC (TYPE A)
0.50' CTB
- 11

0.10' RAC-G
0.10' AC (TYPE A)
0.50' CL2 AB
1.52' CL4 AS
- 12

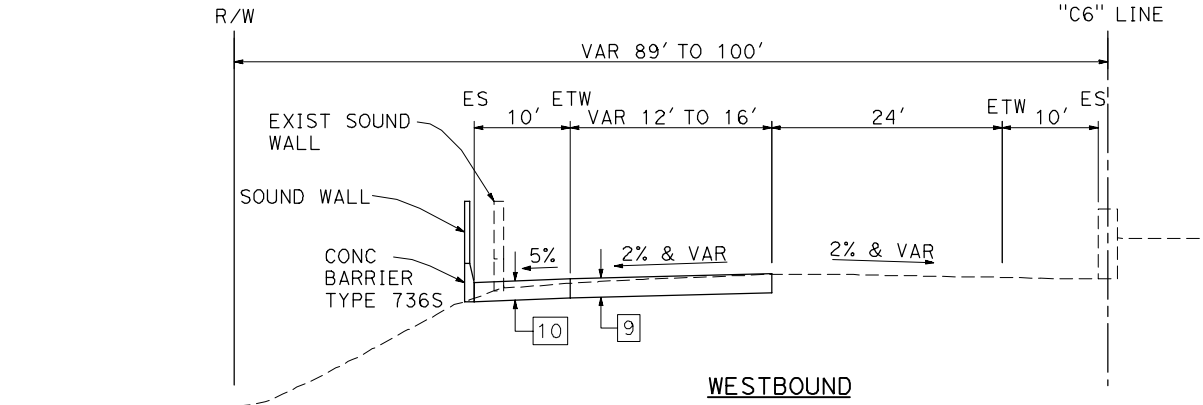
0.10' OGAC
0.15' RAC-G
0.18' AC (TYPE A)
0.67' CTB
0.50' CL4 AS
- 13

0.4' AC (TYPE A)
0.8' CL2 AB
1.3' CL4 AS



"C6" 215+00 TO "C6" 222+32

ROUTE 80



"C6" 213+32 TO "C6" 215+00

ROUTE 80

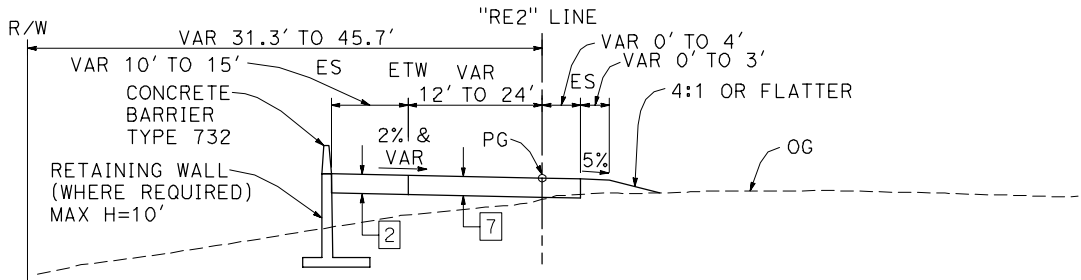
35% SUBMITTAL
TYPICAL CROSS SECTIONS

NO SCALE

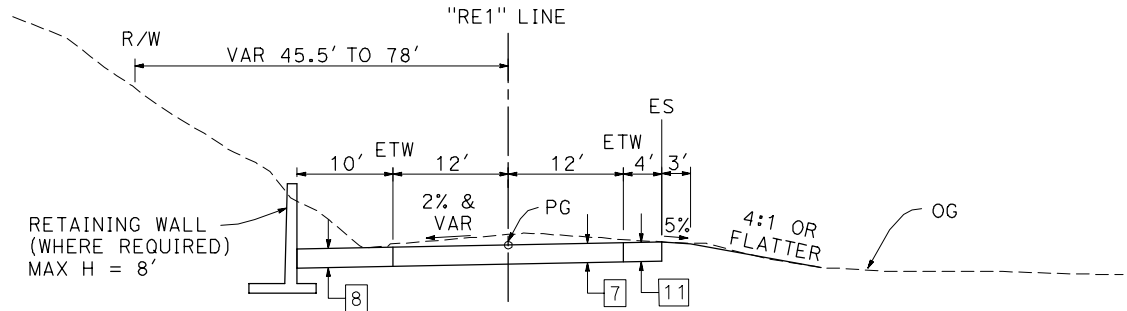
X-1

NOTE:

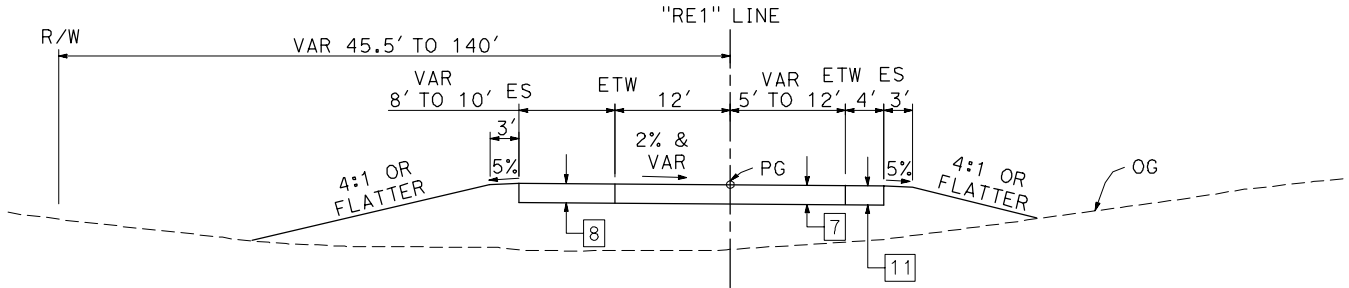
FOR NOTES, ABBREVIATIONS, AND LEGEND, SEE TYPICAL CROSS SECTIONS SHEET X-1.



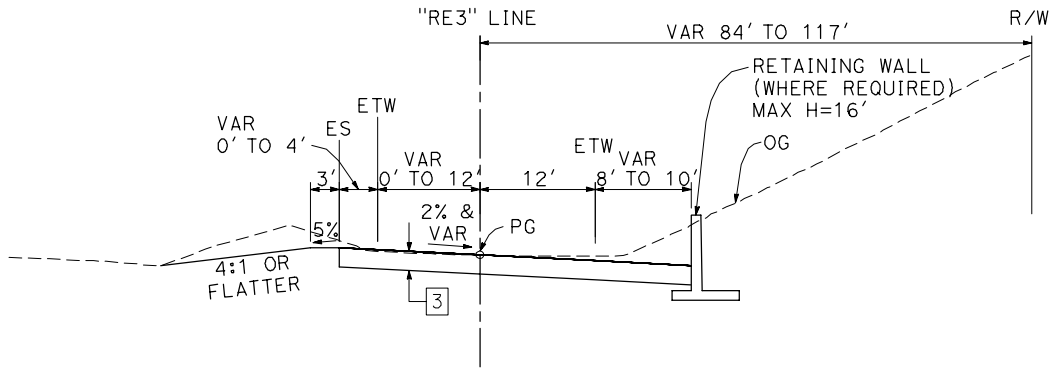
"RE2" 225+60 TO "RE2" 226+39
WB I-80 ON-RAMP FROM REDWOOD ST



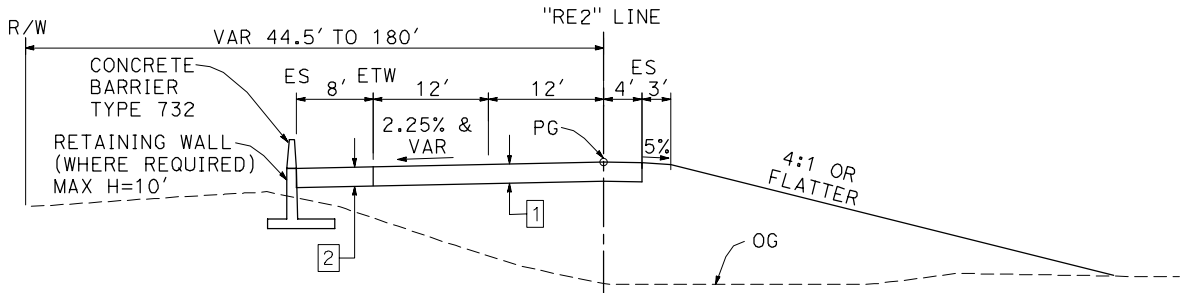
"RE1" 234+90 TO "RE1" 236+50
WB I-80 OFF-RAMP TO REDWOOD ST



"RE1" 236+50 TO "RE1" 237+15
"RE1" 233+78 TO "RE1" 234+90
WB I-80 OFF-RAMP TO REDWOOD ST



"RE3" 225+20 TO "RE3" 227+50
EB I-80 OFF-RAMP TO REDWOOD PKWY



"RE2" 226+39 TO "RE2" 230+00
WB I-80 ON-RAMP FROM REDWOOD ST

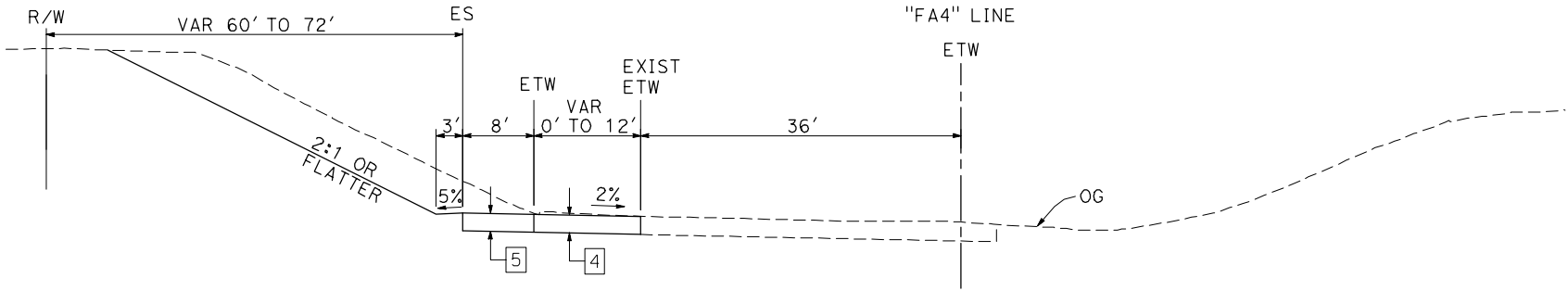
35% SUBMITTAL
TYPICAL CROSS SECTIONS

NO SCALE

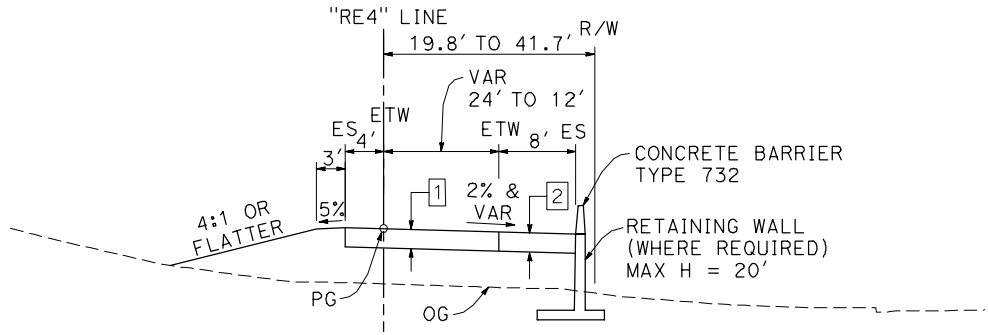
X-3

NOTE:

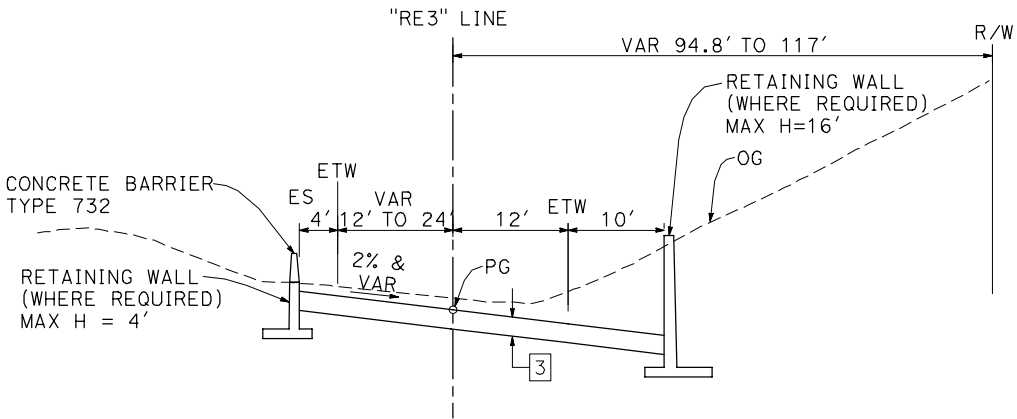
FOR NOTES, ABBREVIATIONS, AND LEGEND, SEE TYPICAL CROSS SECTIONS SHEET X-1.



WB SR-37 OFF-RAMP TO FAIRGROUNDS DR



EB I-80 ON-RAMP FROM REDWOOD PKWY



EB I-80 OFF-RAMP TO REDWOOD PKWY

35% SUBMITTAL
TYPICAL CROSS SECTIONS

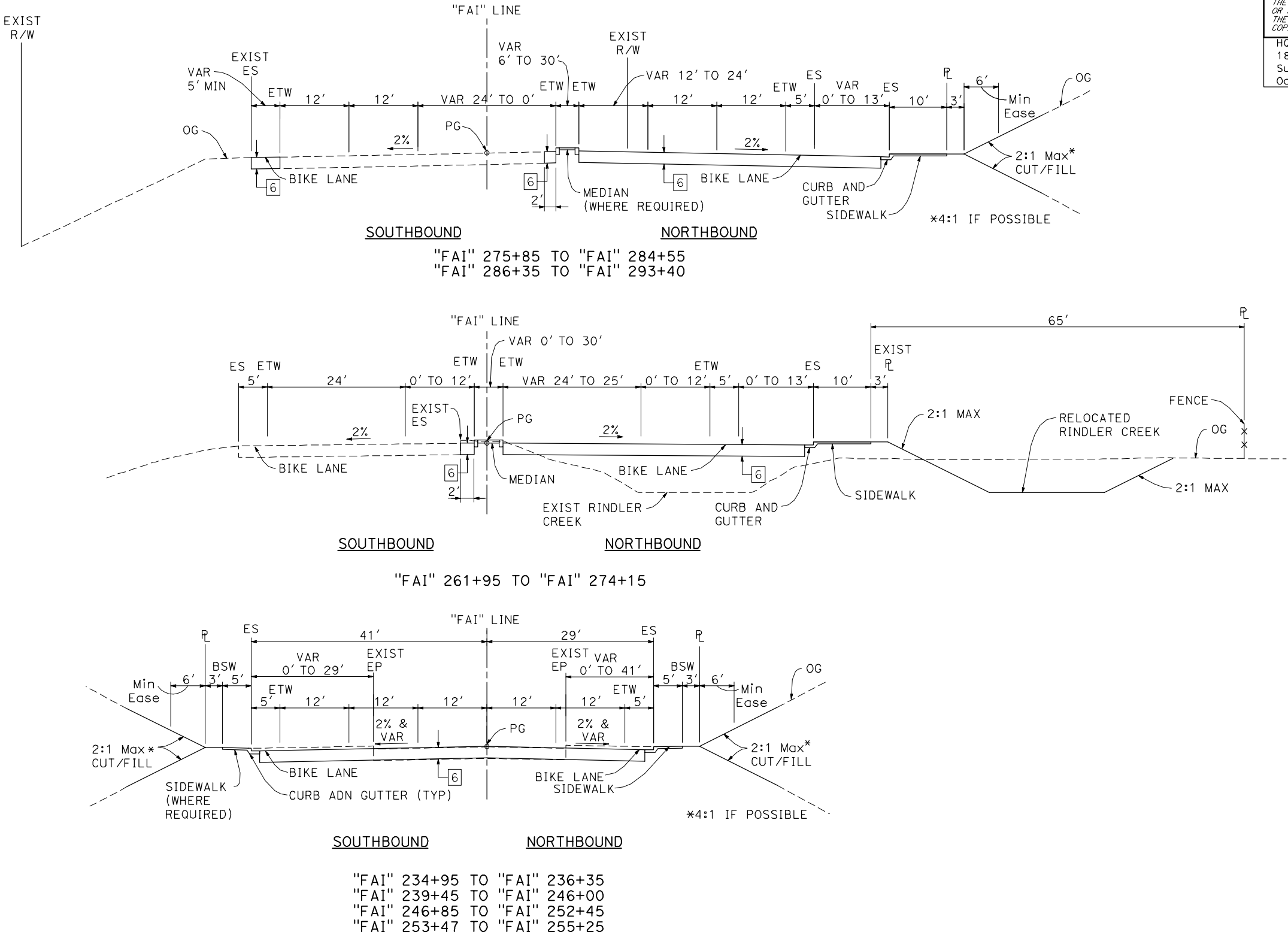
NO SCALE

X - 4

Dist	COUNTY	LOCATION CODE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
04	SoI	37, 80	4.0/4.9, 10.6/11.2		
REGISTERED CIVIL ENGINEER DATE					
PLANS APPROVAL DATE					
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NOTE:
FOR NOTES, ABBREVIATIONS, AND LEGEND, SEE TYPICAL CROSS SECTIONS SHEET X-1.



FAIRGROUNDS DRIVE

35% SUBMITTAL
TYPICAL CROSS SECTIONS

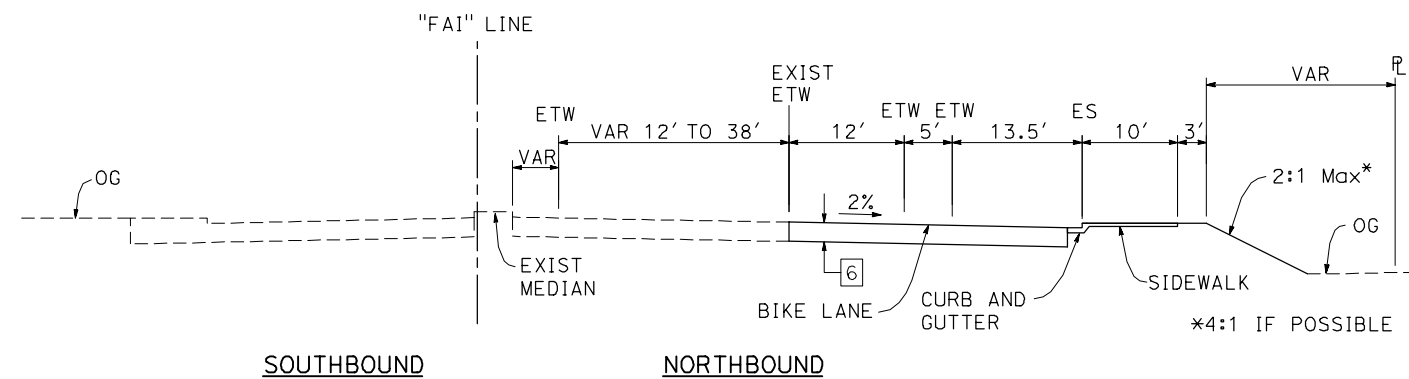
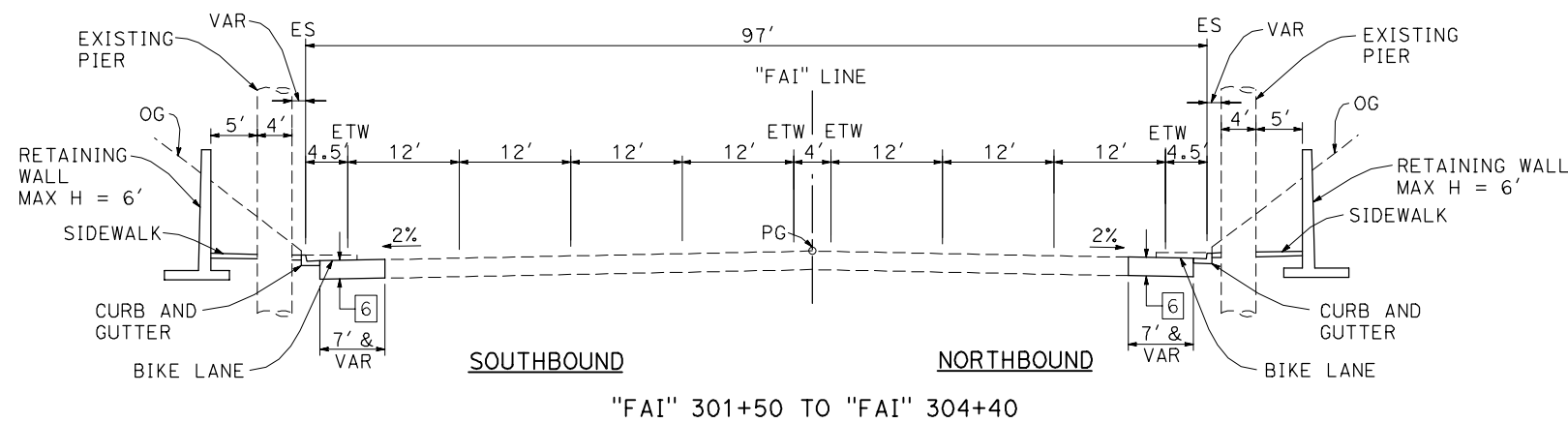
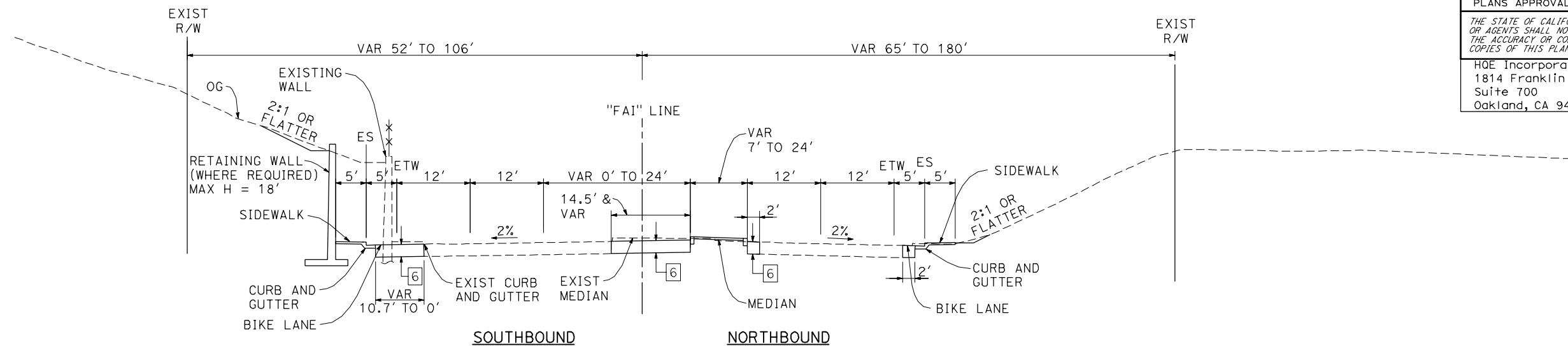
NO SCALE

DIST	COUNTY	LOCATION CODE	POST MILES TOTAL PROJECT	SHEET NO.	TOTAL SHEETS
04	SoI	37, 80	4.0/4.9, 10.6/11.2		
REGISTERED CIVIL ENGINEER DATE					
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Dist	COUNTY	LOCATION CODE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
04	SoI	37, 80	4.0/4.9, 10.6/11.2		
REGISTERED CIVIL ENGINEER DATE					
PLANS APPROVAL DATE					
<p><i>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF ELECTRONIC COPIES OF THIS PLAN SHEET.</i></p>					
HOE Incorporated 1814 Franklin Street Suite 700 Oakland, CA 94612			Solano Transportation Authority One Harbor Center, Suite 130 Suisun City, CA 94585		

NOTE:

FOR NOTES, ABBREVIATIONS, AND LEGEND, SEE TYPICAL CROSS SECTIONS SHEET X-1.



35% SUBMITTAL

TYPICAL CROSS SECTIONS

NO SCALE

X - 7

NOTE:
FOR NOTES, ABBREVIATIONS, AND LEGEND, SEE TYPICAL CROSS SECTIONS SHEET X-1.

Dist	COUNTY	LOCATION CODE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
04	SoI	37, 80	4.0/4.9, 10.6/11.2		

REGISTERED CIVIL ENGINEER DATE

PLANS APPROVAL DATE

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VAR 0' TO 13.5'

VAR 5' TO 7'

VAR 4' TO 5'

ETW ETW

24'

1.5%

VAR 11' TO 12'

12'

VAR 12' TO 12.8'

ETW ES

1.5%

VAR 5.8' TO 7'

OG

VAR 2:1 OR FLATTER*

CURB AND GUTTER

SIDEWALK

*1:1 SLOPE AT ABUTMENT

WESTBOUND

EASTBOUND

"RED" 231+60 TO "RED" 232+50
"RED" 232+75 TO "RED" 235+40
REDWOOD PKWY

2:1 MAX FILL (WHERE REQUIRED)

ES ETW

5' 5'

VAR 0' TO 12'

24'

1%

SIDEWALK

CURB AND GUTTER

13

"RED" LINE

24'

24'

24'

VAR 0' TO 12'

ETW ES

1.5%

13

CONCRETE BARRIER (WHERE REQUIRED) TYPE 732

RETAINING WALL (WHERE REQUIRED) MAX H = 12'

WESTBOUND

EASTBOUND

"RED" 225+70 TO "RED" 227+80
REDWOOD ST

2:1 MAX FILL (WHERE REQUIRED)

ETW

5' 13.5'

12'

VAR

VAR 0' TO 24'

36'

ETW ES

5'

VAR 0' TO 5'

SIDEWALK

CONC BARRIER (WHERE REQUIRED) TYPE 736

RETAINING WALL (WHERE REQUIRED) MAX H = 12'

OG

1.5%

13

SIDEWALK (WHERE REQUIRED)

CURB AND GUTTER (TYP)

1%

"RED" LINE

WESTBOUND

EASTBOUND

"RED" 220+50 TO "RED" 223+00
REDWOOD ST

35% SUBMITTAL
TYPICAL CROSS SECTIONS

NO SCALE

X - 8

BORDER LAST REVISED 3/1/2007

RELATIVE BORDER SCALE IS IN INCHES

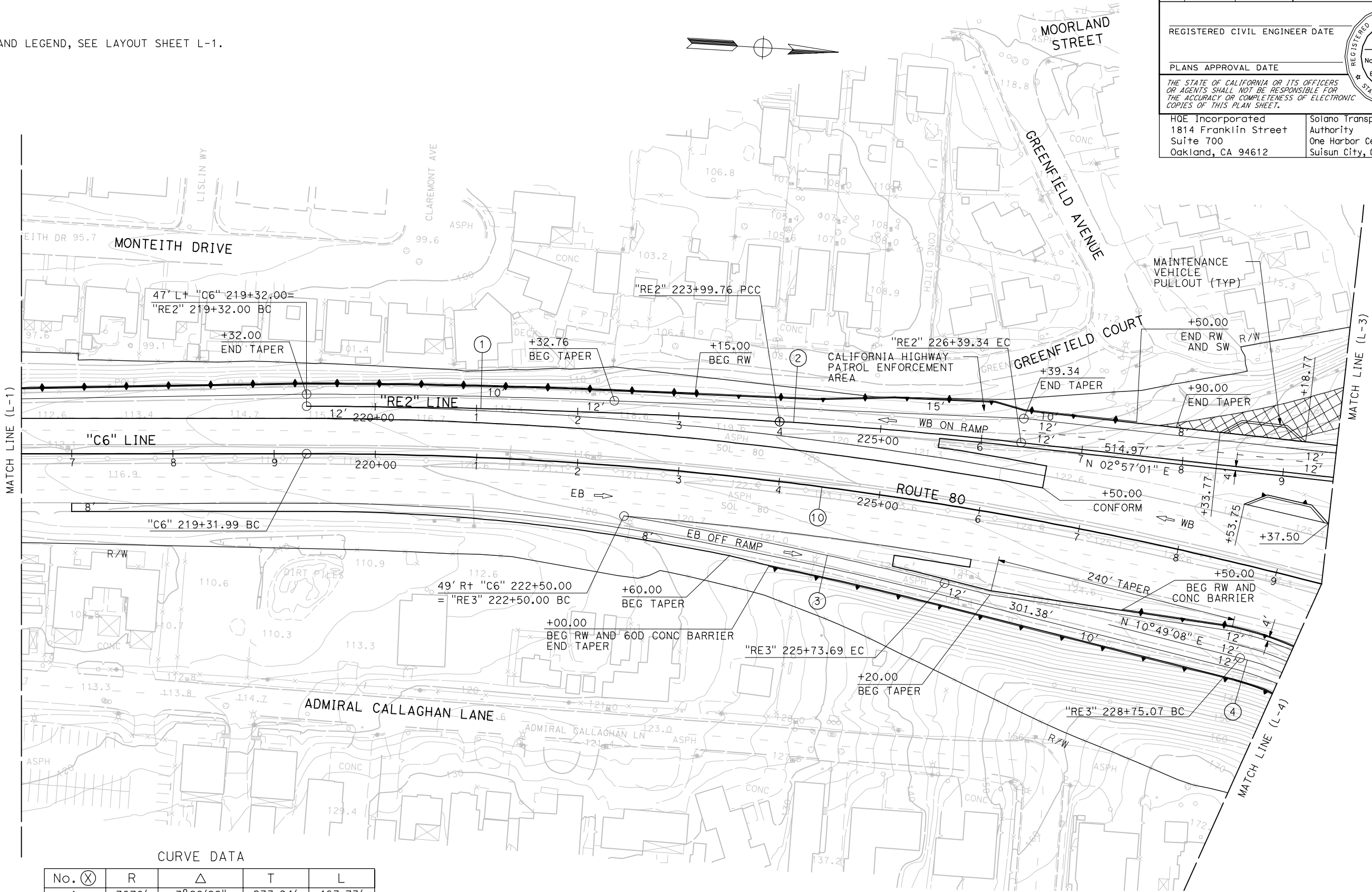
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DGN FILE => \$REQUEST

CU 00000

EA 4A4410

LAST REVISION DATE PLOTTED => \$DATE
00-00-00 TIME PLOTTED => \$TIME

NOTE:
FOR NOTES AND LEGEND, SEE LAYOUT SHEET L-1.



CURVE DATA				
No. (X)	R	Δ	T	L
1	7676'	3°29'29"	233.94'	467.73'
2	5000'	2°44'43"	119.81'	239.58'
3	3900'	4°45'19"	161.94'	323.69'
4	252'	45°12'43"	104.93'	198.85'
10	3965'	30°17'17"	1073.11'	2096.00'

Dist

COUNTY

LOCATION CODE

POST MILES
TOTAL PROJECT

SHEET
No.

TOTAL
SHEETS

04

Soi

37, 80

4.0/4.9,
10.6/11.2

REGISTERED CIVIL ENGINEER DATE

PLANS APPROVAL DATE

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REGISTERED PROFESSIONAL ENGINEER

No.

Exp.

CIVIL

STATE OF CALIFORNIA

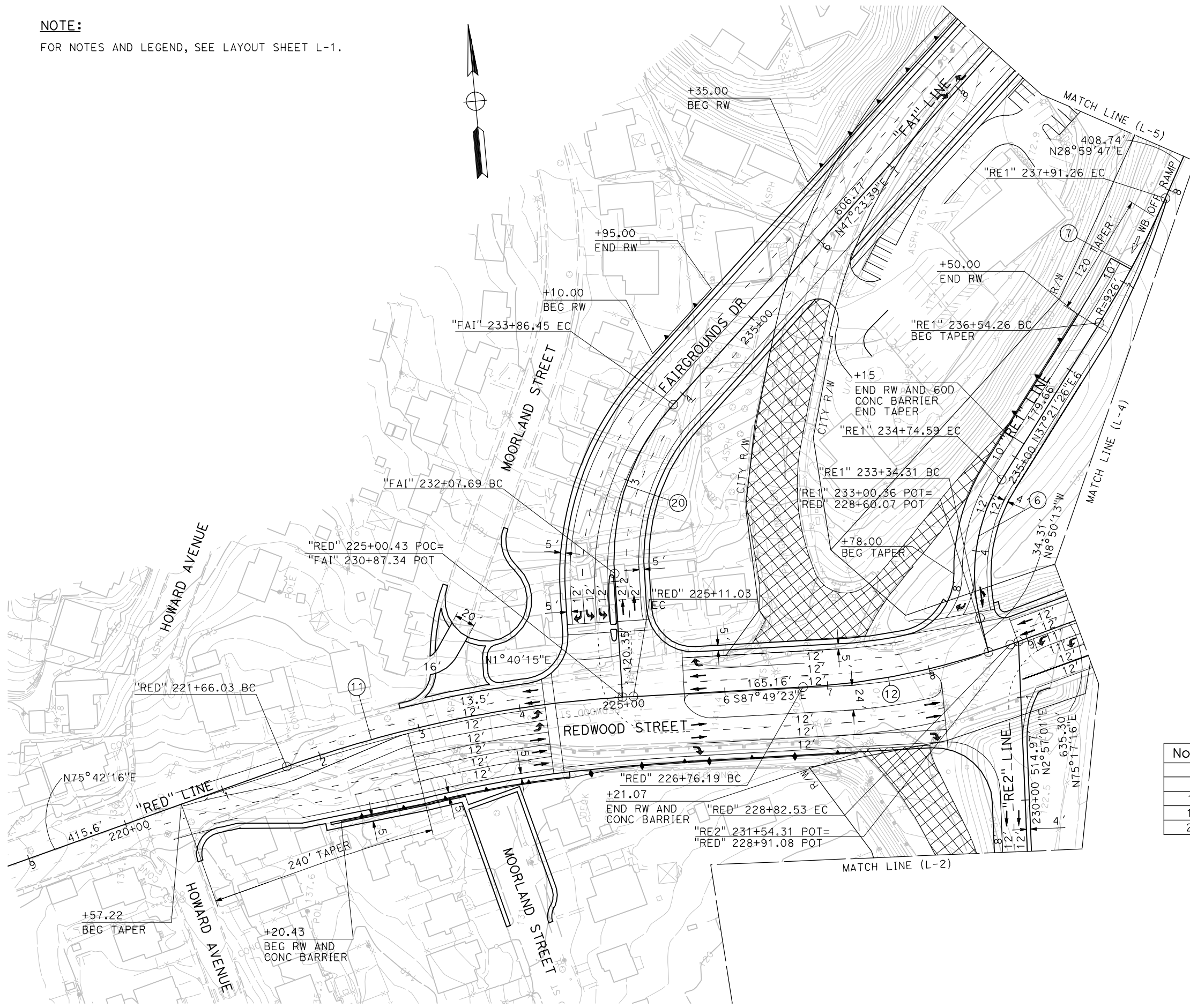
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Suite 700
Oakland, CA 94612

Solano Transportation
Authority
One Harbor Center, Suite 130
Suisun City, CA 94585

35% SUBMITTAL
LAYOUT

SCALE: 1" = 50'

NOTE:
FOR NOTES AND LEGEND, SEE LAYOUT SHEET L-1.



Dist	COUNTY	LOCATION CODE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
04	SoI	37, 80	4.0/4.9, 10.6/11.2		

REGISTERED CIVIL ENGINEER DATE

PLANS APPROVAL DATE

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REGISTERED PROFESSIONAL ENGINEER

No. _____

Exp. _____

CIVIL

STATE OF CALIFORNIA

CURVE DATA				
No. (X)	R	Δ	T	L
6	174'	46°11'39"	74.21'	140.29'
7	926'	8°21'39"	67.68'	135.13'
11	1200'	16°28'21"	173.70'	345.00'
12	700'	16°53'21"	103.92'	147.39'
20	224'	45°43'24"	94.44'	178.76'

Dist	COUNTY	LOCATION CODE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
04	Soi	37, 80	4.0/4.9, 10.6/11.2		

REGISTERED CIVIL ENGINEER DATE

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Authority
One Harbor Center, Suite 130
Suisun City, CA 94585

CURVE DATA				
No. ⓧ	R	Δ	T	L
4	252'	45°12'43"	104.93'	198.85'
5	155'	55°44'35"	81.97'	150.80'
8	512'	33°26'48"	153.83'	298.88'
10	3965'	30°17'17"	1073.11'	2096.00'
13	790'	31°25'44"	222.27'	433.35'
14	1150'	9°6'40"	91.63'	182.87'

35% SUBMITTAL

LAYOUT

SCALE: 1" = 50'

L - 4

BORDER LAST REVISED 3/1/2007

RELATIVE BORDER SCALE
IS IN INCHES

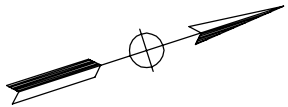
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DGN FILE => \$REQUEST

CU 00000

E-FIS 0400020584

LAST REVISION DATE PLOTTED => \$DATE
00-00-00 TIME PLOTTED => \$TIME

NOTE:
FOR NOTES AND LEGEND, SEE LAYOUT SHEET L-1.



Dist	COUNTY	LOCATION CODE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
04	SoI	37, 80	4.0/4.9, 10.6/11.2		

REGISTERED CIVIL ENGINEER DATE

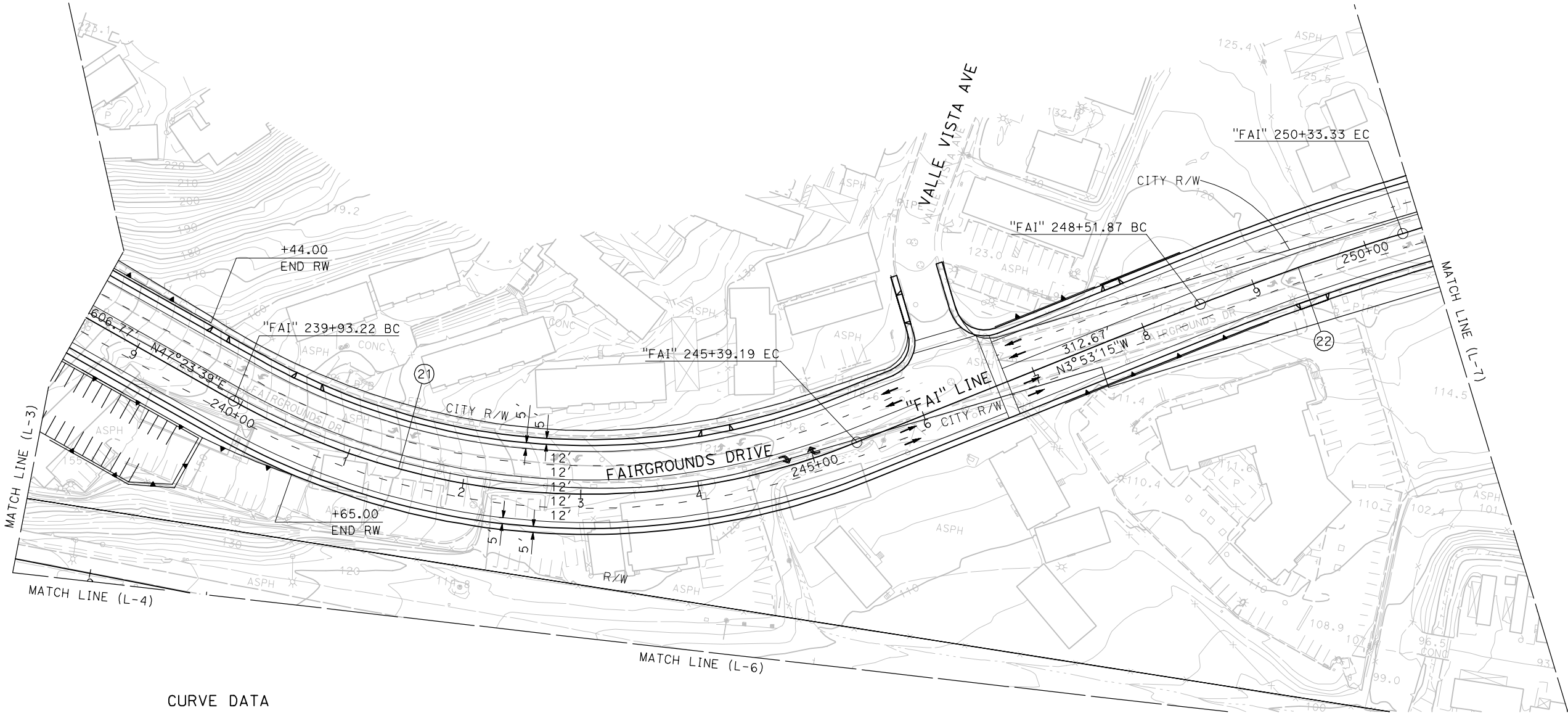
PLANS APPROVAL DATE

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REGISTERED PROFESSIONAL ENGINEER
No. _____
Exp. _____
CIVIL
STATE OF CALIFORNIA



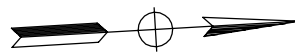
CURVE DATA				
No. (X)	R	Δ	T	L
21	610'	51°16'54"	292.80'	545.97'
22	2000'	5°11'55"	90.80'	181.47'

**35% SUBMITTAL
LAYOUT**

SCALE: 1" = 50'

L - 5

NOTE:
FOR NOTES AND LEGEND, SEE LAYOUT SHEET L-1.



Dist	COUNTY	LOCATION CODE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
04	SoI	37, 80	4.0/4.9, 10.6/11.2		

REGISTERED CIVIL ENGINEER DATE

PLANS APPROVAL DATE

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CURVE DATA				
No. ⓧ	R	Δ	T	L
23	1959'	2°37'36"	44.91'	89.81'
24	2041'	8°33'32"	152.73'	304.89'

35% SUBMITTAL
LAYOUT

SCALE: 1" = 50'

L - 7

BORDER LAST REVISED 3/1/2007

RELATIVE BORDER SCALE IS IN INCHES

0 1 2 3

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DGN FILE => \$REQUEST

CU 00000

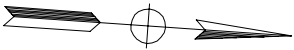
E-FIS 0400020584

LAST REVISION
00-00-00
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TIME PLOTTED => \$TIME

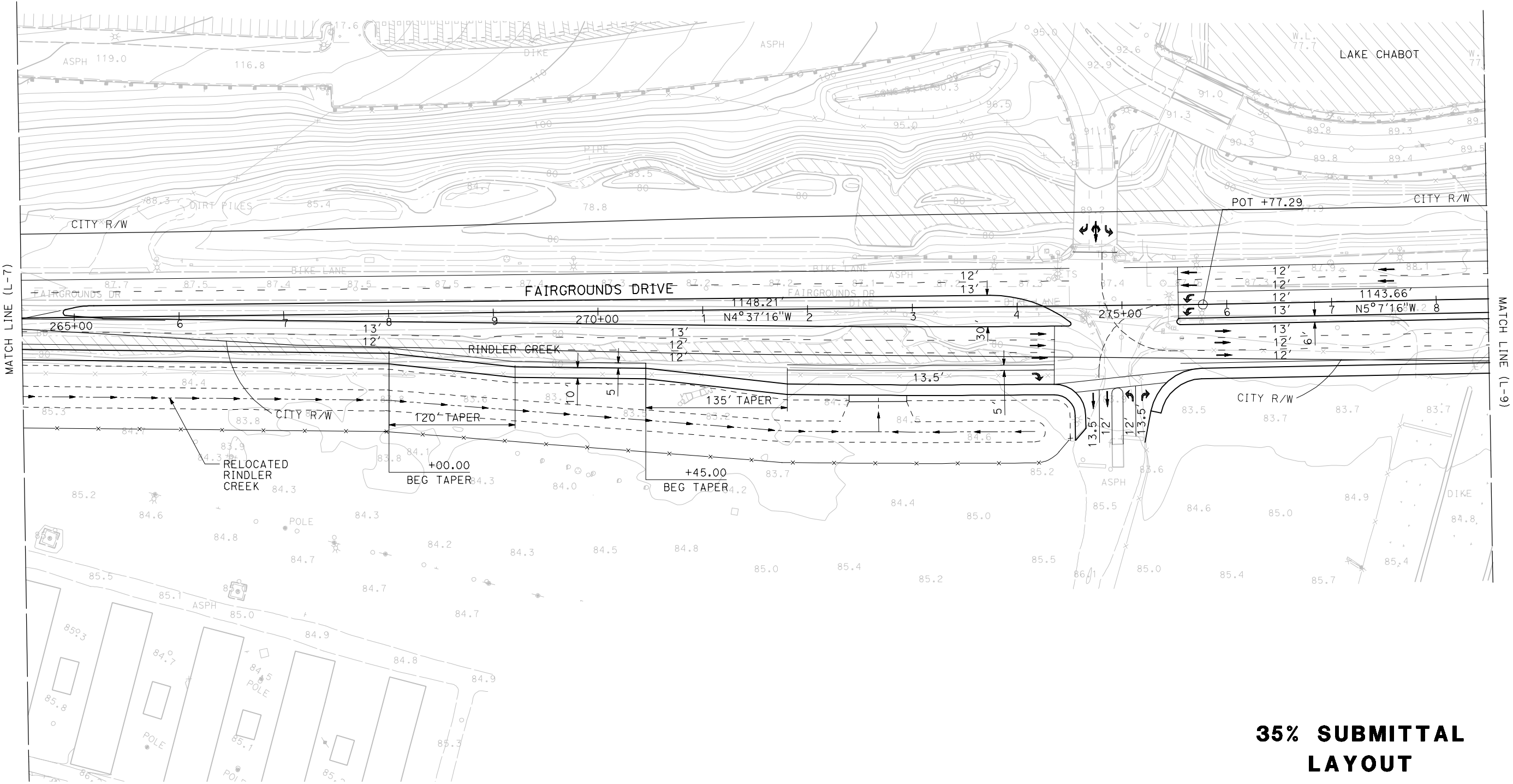
x			FUNCTIONAL SUPERVISOR	REVISED BY	DATE	REVISED BY	DATE			
x			CALCULATED- DESIGNED BY	CHECKED BY						
x	FUNCTIONAL SUPERVISOR									
x	STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION									
	Caltrans									

NOTE:

FOR NOTES AND LEGEND, SEE LAYOUT SHEET L-1.



Dist	COUNTY	LOCATION CODE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
04	SoI	37, 80	4.0/4.9, 10.6/11.2		
REGISTERED CIVIL ENGINEER DATE					
PLANS APPROVAL DATE					
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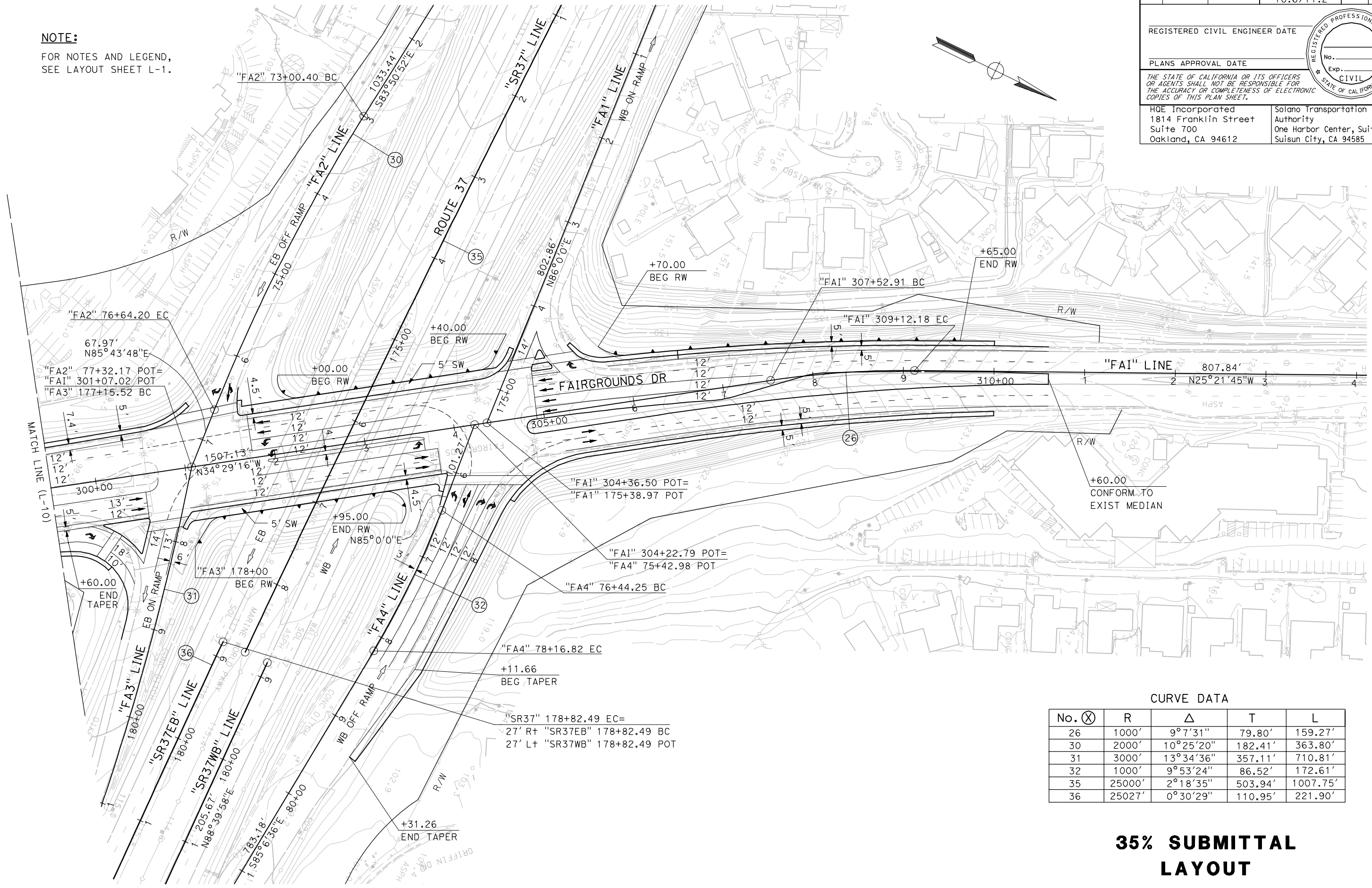
35% SUBMITTAL
LAYOUT

SCALE: 1" = 50'

L - 8



NOTE:
FOR NOTES AND LEGEND,
SEE LAYOUT SHEET L-1.



Dist	COUNTY	LOCATION CODE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
04	SoI	37, 80	4.0/4.9, 10.6/11.2		

REGISTERED CIVIL ENGINEER DATE

PLANS APPROVAL DATE

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Suisun City, CA 94585

REGISTERED PROFESSIONAL ENGINEER
No. _____
Exp. _____
CIVIL
STATE OF CALIFORNIA

CURVE DATA				
No. ⊗	R	Δ	T	L
26	1000'	9° 7' 31"	79.80'	159.27'
30	2000'	10° 25' 20"	182.41'	363.80'
31	3000'	13° 34' 36"	357.11'	710.81'
32	1000'	9° 53' 24"	86.52'	172.61'
35	25000'	2° 18' 35"	503.94'	1007.75'
36	25027'	0° 30' 29"	110.95'	221.90'

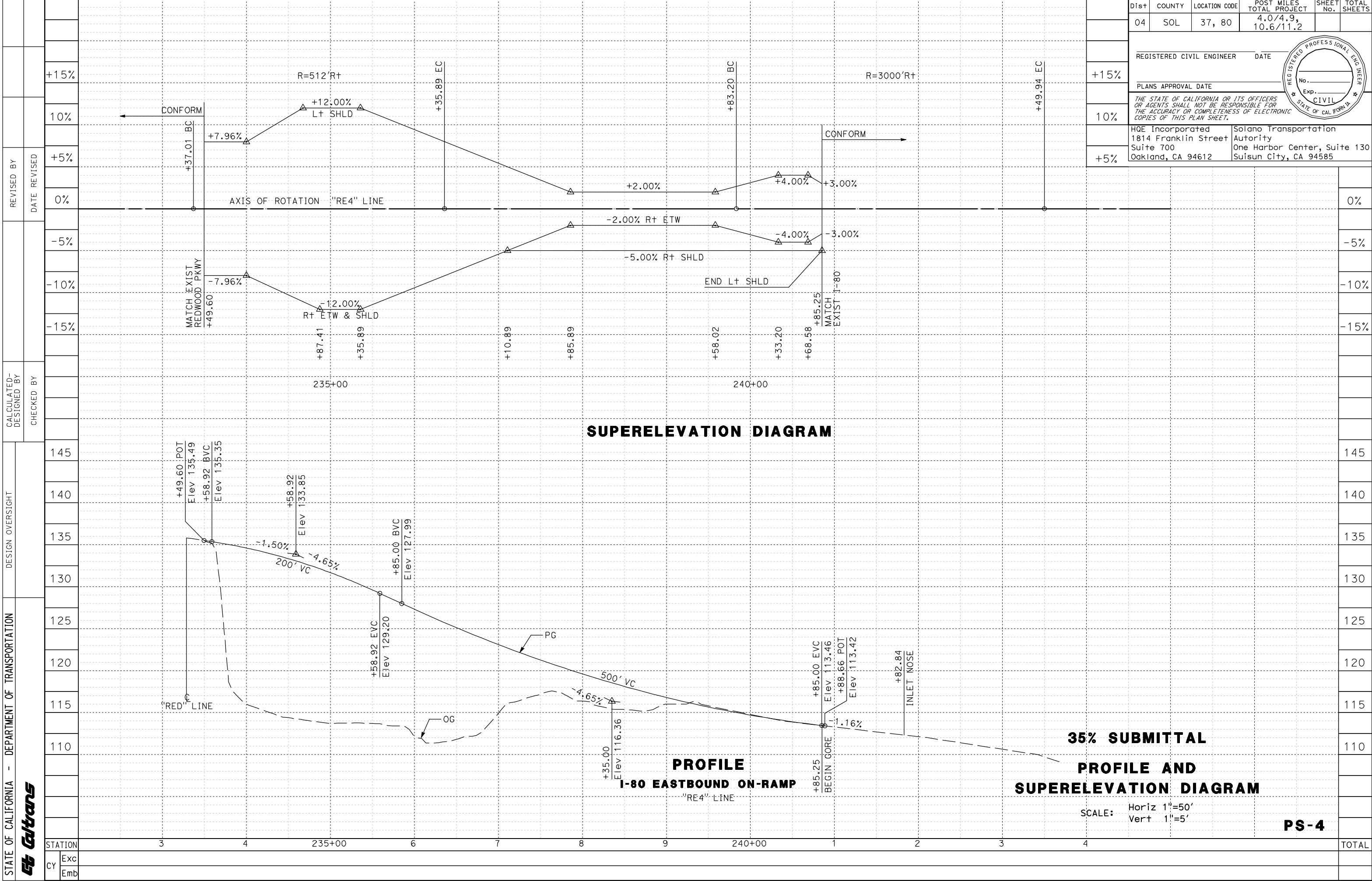
35% SUBMITTAL
LAYOUT

SCALE: 1" = 50'

[illegible]

[illegible]

DATE PLOTTED => \$DATE	LAST REVISION
TIME PLOTTED => \$TIME	00-00-00



[illegible]

DATE PLOTTED => \$DATE	LAST REVISION
TIME PLOTTED => \$TIME	00-00-00

DIST	COUNTY	LOCATION CODE	POST MILES TOTAL PROJECT	SHEET NO.	TOTAL SHEETS
04	SOL	37, 80	4.0/4.9, 10.6/11.2		
REGISTERED CIVIL ENGINEER			DATE		
PLANS APPROVAL DATE					
<small>THE STATE OF CALIFORNIA OR ITS OFFICERS OR AGENTS SHALL NOT BE RESPONSIBLE FOR THE ACCURACY OR COMPLETENESS OF ELECTRONIC COPIES OF THIS PLAN SHEET.</small>					
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+10%					
+5%					
0%					
-5%					
-10%					
190					
185					
180					
175					
170					
165					
160					
155					
150					
145					
140					
135					
35% SUBMITTAL					
PROFILE AND SUPERELEVATION DIAGRAM					
SCALE: Horiz 1"=50' Vert 1"=5'					
PS-6					
LAST REVISION IS THE LATEST					

DATE PLOTTED => \$DATE
TIME PLOTTED => \$TIME
00-00-00

Attachment C

Cost Estimate

**PROJECT REPORT
PRELIMINARY COST ESTIMATE**

District-County-Route	04-SOL-80,04-SOL-37
PM	4.0/4.9, 10.6/11.2
EA	4A4410

PROJECT DESCRIPTION:

Limits Route 80/Redwood Parkway to Route 37
in the City of Vallejo

Proposed Improvement (Scope) Redwood Parkway Interchange Modifications
Fairground widening from Redwood Interchange to Route 37
and Route 37/Fairgrounds Drive Interchange improvements

SUMMARY OF PROJECT COST ESTIMATE

TOTAL ROADWAY ITEMS	\$	31,480,000
TOTAL STRUCTURE ITEMS	\$	-
SUBTOTAL CONSTRUCTION COSTS	\$	31,480,000
TOTAL RIGHT OF WAY ITEMS	\$	16,473,000
TOTAL PROJECT CAPITAL OUTLAY COSTS	\$	47,953,000

(Costs in 2012 Dollars)

Review by

(Signature)

Approved by

(Signature)

Date

8-17-12

Phone No.

510-763-4895

Note: Unit Costs were re-evaluated on January 28, 2015 to provide updated cost estimates. The results of the update indicate a reduction of 6.2%. Therefore, it is recommended that the unit costs applied in January 2012 for the Draft Project Report remain as is for the Final Project Report.

Heidi M. Ouren
Heidi M. Ouren, P.E.
Project Manager

5/21/15

**PROJECT REPORT
PRELIMINARY COST ESTIMATE**

District-County-Route	04-SOL-80,04-SOL-37
PM	4.0/4.9, 10.6/11.2
EA	4A4410

I. ROADWAY ITEMS

<u>Section 1 Earthwork</u>	<u>Quantity</u>	<u>Unit</u>	<u>Unit Price</u>	<u>Item Cost</u>	<u>Section Cost</u>
Roadway Excavation	75000	CY	\$20.00	\$1,500,000	
Roadway Excavation(Y-1)	2000	CY	\$33.00	\$70,000	
Imported Borrow		CY		\$0	
Clearing & Grubbing		LS		\$0	
Develop Water Supply		LS		\$0	
Cold Plane AC	38100	SY	\$1.50	\$60,000	
Subtotal Earthwork					\$1,630,000

Section 2 Pavement Structural Section*

Cement Treated Base	3000	CY	\$90.00	\$270,000	
ATPB	700	CY	\$180.00	\$130,000	
Class 4 Aggregate Subbase	8000	CY	\$55.00	\$440,000	
Class 2 Aggregate Base	12000	CY	\$60.00	\$720,000	
Asphalt Concrete (Type A)	1000	TON	\$200.00	\$200,000	
Asphalt Concrete (Type B)	20100	TON	\$125.00	\$2,510,000	
OGAC	800	TON	\$140.00	\$110,000	
RAC-G	1100	TON	\$150.00	\$170,000	
CL 1 PERM MTL	16000	CY	\$50.00	\$800,000	
Subtotal Pavement Structural Section					\$5,350,000

Section 3 Drainage

Large Drainage Facilities	1	LS	\$236,250.00	\$240,000	
Storm Drains	1	LS	\$1,516,000.00	\$1,520,000	
Subtotal Drainage					\$1,760,000

*Reference sketch showing typical pavement structural section elements of the roadway. Include (if available) T.I., R-Value and date when tests were performed.

NOTE: Extra lines are provided for items not listed, use additional lines as appropriate.

**PROJECT REPORT
PRELIMINARY COST ESTIMATE**

District-County-Route	04-SOL-80,04-SOL-37
PM	4.0/4.9, 10.6/11.2
EA	4A4410

<u>Section 4 Specialty Items</u>	<u>Quantity</u>	<u>Unit</u>	<u>Unit Price</u>	<u>Item Cost</u>	<u>Section Cost</u>
Retaining Walls	50710	SF	\$105.00	\$5,320,000	
Noise Barriers	1	LS	\$660,010.00	\$660,000	
Concrete Barrier (Type 60)	3820	LF	\$65.00	\$250,000	
Concrete Barrier (Type 60C)		LF		\$0	
Highway Planting				\$0	
Replacement Planting	1	LS	\$455,000.00	\$460,000	
Irrigation Modification	1	LS	\$152,000.00	\$150,000	
Relocate Private Irrigation Facilities				\$0	
Erosion Control	1	LS	\$152,000.00	\$150,000	
Slope Protection				\$0	
Construction Site BMP	1	LS	\$455,000.00	\$460,000	
Hazardous Waste Mitigation		LS		\$0	
Work - See Roadway Ex. (Type Y)					
Environmental Mitigation	0.12	Acre	\$500,000.00	\$60,000	
Resident Engineer Office Space				\$0	
Prepare SWPPP & WPCP				\$0	
Treatment BMP	1	LS	\$758,000.00	\$760,000	
Landscaping/Irrigation (normally separate project)				\$0	
Temporary Railing (Type K)	14700	LF	\$14.00	\$210,000	
				Subtotal Specialty Items	\$8,480,000
<u>Section 5 Traffic Items</u>					
Lighting		LS		\$0	
Traffic Delineation Items				\$0	
Traffic Signals	5	EA	\$200,000	\$1,000,000	
Traffic Signals (Mods)	2	EA	\$100,000	\$200,000	
Overhead Sign Structures	2	EA	\$100,000	\$200,000	
Roadside Signs				\$0	
Traffic Control Systems (Temporary)	1	LS	\$1,296,000.00	\$1,300,000	
TMP	1	LS	\$220,000.00	\$220,000	
Transportation Management (Permanent)		LS		\$0	
Ramp Metering Systems	2	EA	\$150,000	\$300,000	
				Subtotal Traffic Items	\$3,220,000
				TOTAL SECTIONS 1 thru 5	\$20,440,000

NOTE: Extra lines are provided for items not listed, use additional lines as appropriate.

**PROJECT REPORT
PRELIMINARY COST ESTIMATE**

District-County-Route	04-SOL-80,04-SOL-37
PM	4.0/4.9, 10.6/11.2
EA	4A4410

Section 6 Minor Items

<u>\$20,440,000.00</u>	x	(10%)	=	<u>\$2,044,000</u>
(Subtotal Section 1 thru 5)						

TOTAL MINOR ITEMS	<u>\$2,044,000</u>
-------------------	--------------------

Section 7 Roadway Mobilization

<u>\$22,484,000.00</u>	x	(10%)	=	<u>\$2,250,000</u>
(Subtotal Section 1 thru 6)						

TOTAL ROADWAY MOBILIZATION	<u>\$2,250,000</u>
----------------------------	--------------------

Section 8 Roadway Additions

Supplemental Work

<u>\$22,484,000.00</u>	x	(10%)	=	<u>\$2,250,000</u>
(Subtotal Section 1 thru 6)						

Contingencies

<u>\$22,484,000.00</u>	x	(20%)	=	<u>\$4,500,000</u>
(Subtotal Section 1 thru 6)						

TOTAL ROADWAY ADDITIONS	<u>\$6,750,000</u>
-------------------------	--------------------

TOTAL ROADWAY ITEMS (Subtotal Section 1 thru 8)	<u>\$31,480,000</u>
--	---------------------

Estimate Prepared By	<u>Jeff Leung</u> (Print Name)	Phone#	<u>510-763-4895</u>	Date	<u>1/13/2012</u>
----------------------	-----------------------------------	--------	---------------------	------	------------------

Estimate Checked By	<u>Julia Chuang</u> (Print Name)	Phone#	<u>510-763-4895</u>	Date	<u>1/13/2012</u>
---------------------	-------------------------------------	--------	---------------------	------	------------------

** Use appropriate percentage per Chapter 20.

**PROJECT REPORT
PRELIMINARY COST ESTIMATE**

District-County-Route	<u>04-SOL-80,04-SOL-37</u>
PM	<u>4.0/4.9, 10.6/11.2</u>
EA	<u>4A4410</u>

II. STRUCTURES ITEMS

	Structure (1)	Structure (2)	Structure (3)	
Bridge Name	<u> </u>	<u> </u>	<u> </u>	
Structure Type	<u> </u>	<u> </u>	<u> </u>	
Width (out to out) - (ft)	<u> </u>	<u> </u>	<u> </u>	
Span Lengths - (ft)	<u> </u>	<u> </u>	<u> </u>	
Total Area - (sf)	<u> </u>	<u> </u>	<u> </u>	
Footing Type (pile/spread)	<u> </u>	<u> </u>	<u> </u>	<u> </u>
Cost Per SF	<u> </u>	<u> </u>	<u> </u>	<u> </u>
(incl. 10% mobilization and 25% contingency)				
Total Cost for Structure	<u> </u>	<u> </u>	<u> </u>	
SUBTOTAL STRUCTURAL ITEMS (Sum of Total Cost for Structure)				<u>\$0</u>
Railroad Related Costs:	<u> </u>			<u> </u>
	<u> </u>			<u> </u>
	<u> </u>			<u> </u>
TOTAL STRUCTURES ITEMS (Sum of Structures Items plus Railroad Items)				<u>\$0</u>

COMMENTS:

Estimate Prepared By	<u>Julia Chuang</u>	Phone#	<u>510-763-4895</u>	Date	<u>1/13/2012</u>
	(Print Name)				

NOTE: If appropriate, attach additional pages and backup.

**PROJECT REPORT
PRELIMINARY COST ESTIMATE**

District-County-Route	<u>04-SOL-80,04-SOL-37</u>
PM	<u>4.0/4.9, 10.6/11.2</u>
EA	<u>4A4410</u>

III. RIGHT OF WAY ITEMS

ESCALATED VALUE

A. Acquisition, including excess lands, damages to remainder(s) and Goodwill	<u>\$12,790,000</u>
B. Utility Relocation (State share)	<u>\$988,000</u>
C. Relocation Assistance	<u>\$1,030,000</u>
D. Clearance/Demolition	<u>\$1,665,000</u>
E. Title and Escrow Fees	<u>(included in A. above)</u>

TOTAL RIGHT OF WAY ITEMS	<u>\$16,473,000</u>
(Escalated Value)	

Anticipated Date of Right of Way Certification	_____
(Date to which Values are Escalated)	

F. Construction Contract Work

Brief Description of Work:

Right of Way Branch Cost Estimate for Work * _____

* This dollar amount is to be included in the Roadway and/or Structures Items of Work, as appropriate. Do not include in Right of Way Items.

COMMENTS:

Estimate Prepared By	<u>Julia Chuang</u>	Phone#	<u>510-763-4895</u>	Date	<u>1/13/2012</u>
	(Print Name)				

NOTE: If appropriate, attach additional pages and backup.

Attachment D

Right of Way Data Sheet

RIGHT OF WAY DATA SHEET FOR LOCAL PUBLIC AGENCIES

(Form #)

To: District Office Chief
Division of Right of Way and Land Surveys

Date: 05/15/15
Co. SOL Rte. 80 P. M. 4.0/4.9
Rte. 37 P. M. 10.6/11.2

Attention: District Branch Chief
R/W Local Programs

Expense Authorization 4A4410

Subject: **RIGHT OF WAY DATA SHEET- LOCAL PUBLIC AGENCY SERVICES**

Project Description: I-80/ Redwood Parkway – Fairgoumds Drive Improvements - Ultimate Project

Right of way necessary for the subject project will be the responsibility of Solano Transportation Authority.
The information in this data sheet was developed by HQE Incorporated and reviewed by Contra Costa County.

I. **Right of Way Engineering**

Will right of way engineering be required for this project?

- No
- Yes X (Submit a copy of the *Right of Way Engineering, Surveys and Mapping Services checklist for Special Funded Projects*. This checklist includes but is not limited to the following items.)

- | | |
|-------------------------------|------------|
| • Hard copy (base map) | <u>Yes</u> |
| • Appraisal map | <u>Yes</u> |
| • Acquisition Documents | <u>Yes</u> |
| • Property Transfer Documents | <u>Yes</u> |
| • R/W Record Map | <u>Yes</u> |
| • Record of Survey | <u>Yes</u> |

Remark: All the above documents will be provided at PS&E phase.

II. **Engineering Surveys**

1. Is any surveying or photogrammetric mapping required?

No Yes X (Complete the following)

Photogrammetric Mapping in accordance with CT requirements for Consultant Photogrammetric Mapping has already been prepared and approved. Additional site surveys are anticipated during PS&E.

2. **Datum Requirements**

Yes X Project will adhere to the following criteria:

- Horizontal - datum policy is NAD 83, CA-HPGN, EPOCH 1991.35 and English system of units.
- Vertical - datum policy is NAVD 88.
- Units - metric is not required.

No Provide an explanation on additional page.

3. Will land survey monument perpetuation be scoped into the project, if required?

Yes X

No Provide explanation on additional page.
No monument perpetuation required

RIGHT OF WAY DATA SHEET FOR LOCAL PUBLIC AGENCIES (Cont.)

(Form #)

R/W Data Sheet – Local Public Agencies
Page 2 of 5**III. Parcel Information (Land and Improvements)**

Are there any property rights required within the proposed project limits?

No _____ Yes X (Complete the following.)

	Part Take	Full Take	Estimate \$
A. Number of Vacant Land Parcels	<u> 5 </u>	<u> </u>	\$ <u> 60,000 </u>
B. Number of Single Family Residential Units	<u> 5 </u>	<u> 13 </u>	\$ <u> 3,280,000 </u>
C. Number of Multi-Family Residential Units	<u> </u>	<u> 1 </u>	\$ <u> 400,000 </u>
D. Number of Commercial/Industrial Parcels	<u> 6 </u>	<u> 7 </u>	\$ <u> 9,050,000 </u>
E. Number of Farm/Agricultural Parcels	<u> </u>	<u> </u>	\$ <u> </u>
F. Permanent and/or Temporary Easements	<u> </u>	<u> </u>	\$ <u> </u>
G. Other Parcels (define in "Remarks" section)	<u> </u>	<u> </u>	\$ <u> </u>
Totals	<u> 16 </u>	<u> 21 </u>	\$ <u>12,790,000 </u>

The majority of the properties required for the project improvements are zoned as residential on the City General Plan Zoning map. Some of the areas are zoned as commercial or limited office area. The parcel along Fairgrounds Drive that is within the Solano County Fairgrounds is zoned as public facilities.

Three of the 5 Vacant Land Parcels are owned by the City of Vallejo and Solano County.

IV. Dedications

Are there any property rights which have been acquired, or anticipated will be acquired, through the "dedication" process for the Project?

No X Yes _____ (Complete the following.)

Number of dedicated parcel _____

Have the dedicated parcels(s) been accepted by the municipality involved?

V. Excess Lands/Relinquishments

Are there Caltrans property rights which may become excess lands or potential relinquishment areas?

No _____ Yes X (Provide an explanation on additional page.)
See "Remarks" section for explanation.

RIGHT OF WAY DATA SHEET FOR LOCAL PUBLIC AGENCIES (Cont.)

(Form #)

R/W Data Sheet – Local Public Agencies

Page 3 of 5

VI. Relocation Information

Are relocation displacements anticipated?

No _____ Yes X (Complete the following.)

A.	Number of Single Family Residential Units	<u> 13 </u>	
	Estimated RAP Payments		\$ <u>650,000</u>
B.	Number of Multifamily Residential Units	<u>1 Unit w/4 Relocations</u>	
	Estimated RAP Payments		\$ <u>100,000</u>
C.	Number of Business/Nonprofit	<u> 7 </u>	
	Estimated RAP Payments		\$ <u>280,000</u>
D.	Number of Farms	<u> </u>	
	Estimated RAP Payments		\$ <u> </u>
E.	Other (define in the "Remarks" section)	<u> </u>	
	Estimated RAP Payments		\$ <u> </u>
Totals		<u> 21 </u>	\$ <u>1,030,000</u>

VII. Utility Relocation Information

Do you anticipate any utility facilities or utility rights of way to be affected?

No _____ Yes X (Complete the following.)

Estimated Relocation Expense				
Facility	Owner	State Obligation	Local Obligation	Utility Owner Obligation
A. Electrical	PG&E Distribution	\$	\$240,000	\$240,000
B. Gas	PG&E Distribution	\$	\$215,000	\$215,000
C. Water	City of Vallejo	\$	\$78,000	\$0
D.		\$	\$	\$
E.		\$	\$	\$
F.		\$	\$	\$
Totals		\$ *	\$533,000	\$455,000
Number of facilities			3	

*This amount reflects the estimated total financial obligation by the State.

Any additional information concerning utility involvement on this project? There are a total of 10 separate relocations required among the three facilities listed above.

R/W Data Sheet – Local Public Agencies
Page 4 of 5

VIII. Railroad Information

Are railroad facilities or railroad rights of way affected?

No X Yes (Complete the following.)

Describe railroad facilities or railroad rights of way affected.

Owner's Name	Transverse Crossing	Longitudinal Encroachment
A.		
B.		

Discuss types of agreements and rights required from the railroads. Are grade crossings that require services contracts, or grade separations that require construction and maintenance agreements involved?

IX. Clearance Information

Are there improvements that require clearance?

No _____ Yes X (Complete the following.)

A. Number of structures to be Demolished	21	
Estimated Cost of Demolition		\$ 1,665,000

X. Hazardous Materials/Waste

Are there any site(s) and/or improvement(s) in the Project Limits that are known to contain *hazardous material*? None _____ Yes X (Explain in the "Remarks" section.)

Are there any site(s) and/or improvement(s) in the Project Limits that are suspected to contain *hazardous material*? None _____ Yes X (Explain in the "Remarks" section.)

XI. Project Scheduling

	Proposed lead time		Completion Date
*Preliminary Engineering, Surveys	27	(months)	6/15
*R/W Engineering Submittals	12	(months)	6/16
*R/W Appraisals/Acquisitions	18	(months)	12/16
Proposed Environmental Clearance			6/15
Proposed R/W Certification			5/17

STATE OF CALIFORNIA – DEPARTMENT OF TRANSPORTATION
RIGHT OF WAY DATA SHEET FOR LOCAL PUBLIC AGENCIES (Cont.)
 (Form #)

R/W Data Sheet – Local Public Agencies
 Page 5 of 5

XII. Proposed Funding

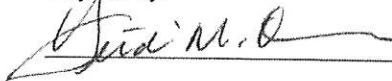
	Local	State	Federal	Other
Acquisition	\$12,790,000			
Utilities	\$988,000			
Relocation Assistance Program	\$1,030,000			
Clearance and Demolition	\$1,665,000			
Cost (R/W Support, Eng. Appraisals, etc.)	Included in PS&E Cost			
TOTAL	\$16,473,000			

XII. Remarks

Project construction areas consist of existing or previous locations of gasoline service stations, automobile service business. The Initial Site Assessment has identified hazardous materials within these parcels. One parcel containing a gas station was also not available for subsurface exploration and will require testing prior to acquisition.

The area bounded by the proposed I-80 EB on-ramp from Redwood Pkwy and Admiral Callaghan Ln that is currently the termini of the I-80 EB on and off hook ramps to Admiral Callaghan Ln, may become excess land or potential relinquishment areas.

Project Sponsor Consultant
 Prepared by:

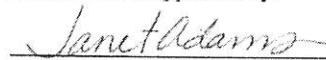


Heidi Ouren – HOE Incorporated

5/19/15

Date

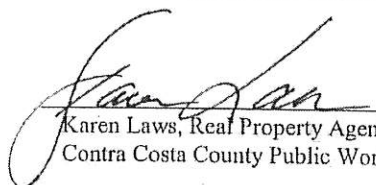
Project Sponsor
 Reviewed and Approved by:



Janet Adams – Solano Transportation Authority

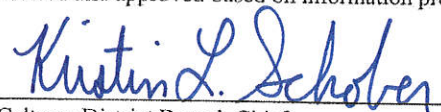
Date

Reviewed and approved by Project Sponsor Right of Way Specialist


 Karen Laws, Real Property Agent
 Contra Costa County Public Works Department

5/19/15
 Date

Caltrans
 Reviewed and approved based on information provided to date:


 Caltrans District Branch Chief
 Local Programs
 Division of Right of Way

6/4/15
 Date

Attachment E

Existing and Forecasted Traffic Data

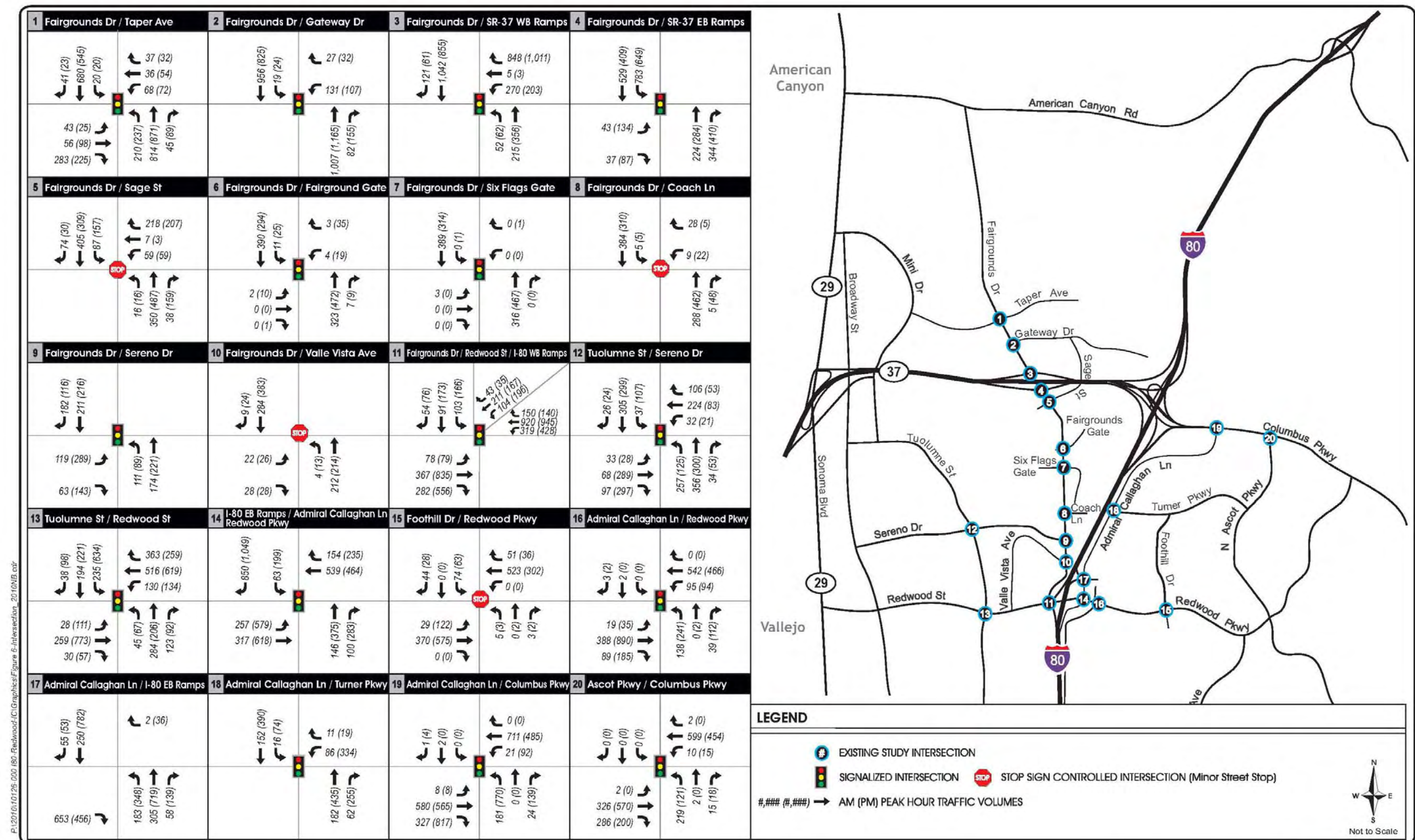


Figure 23 2010 intersection volumes

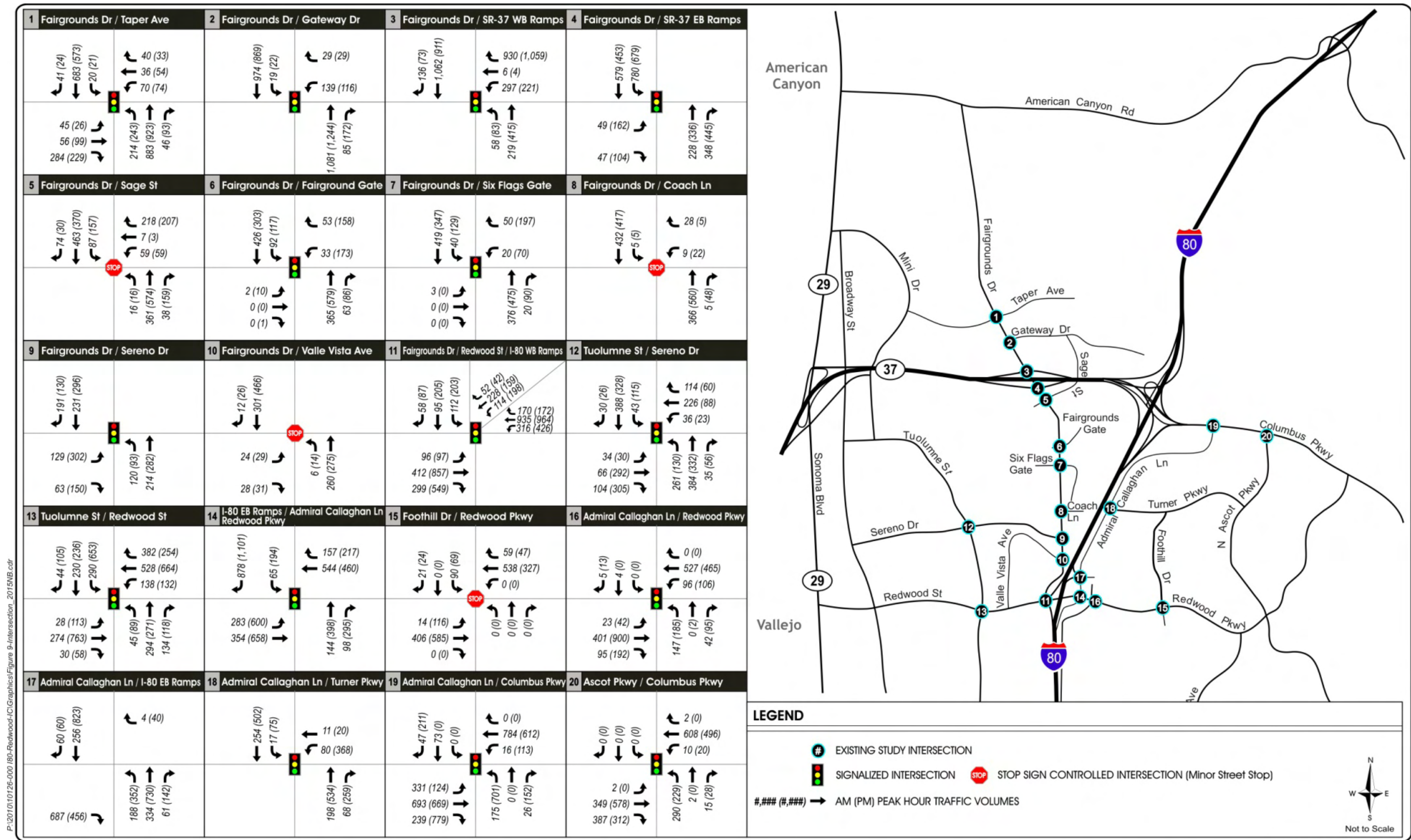


Figure 24 2015 no build intersection volumes

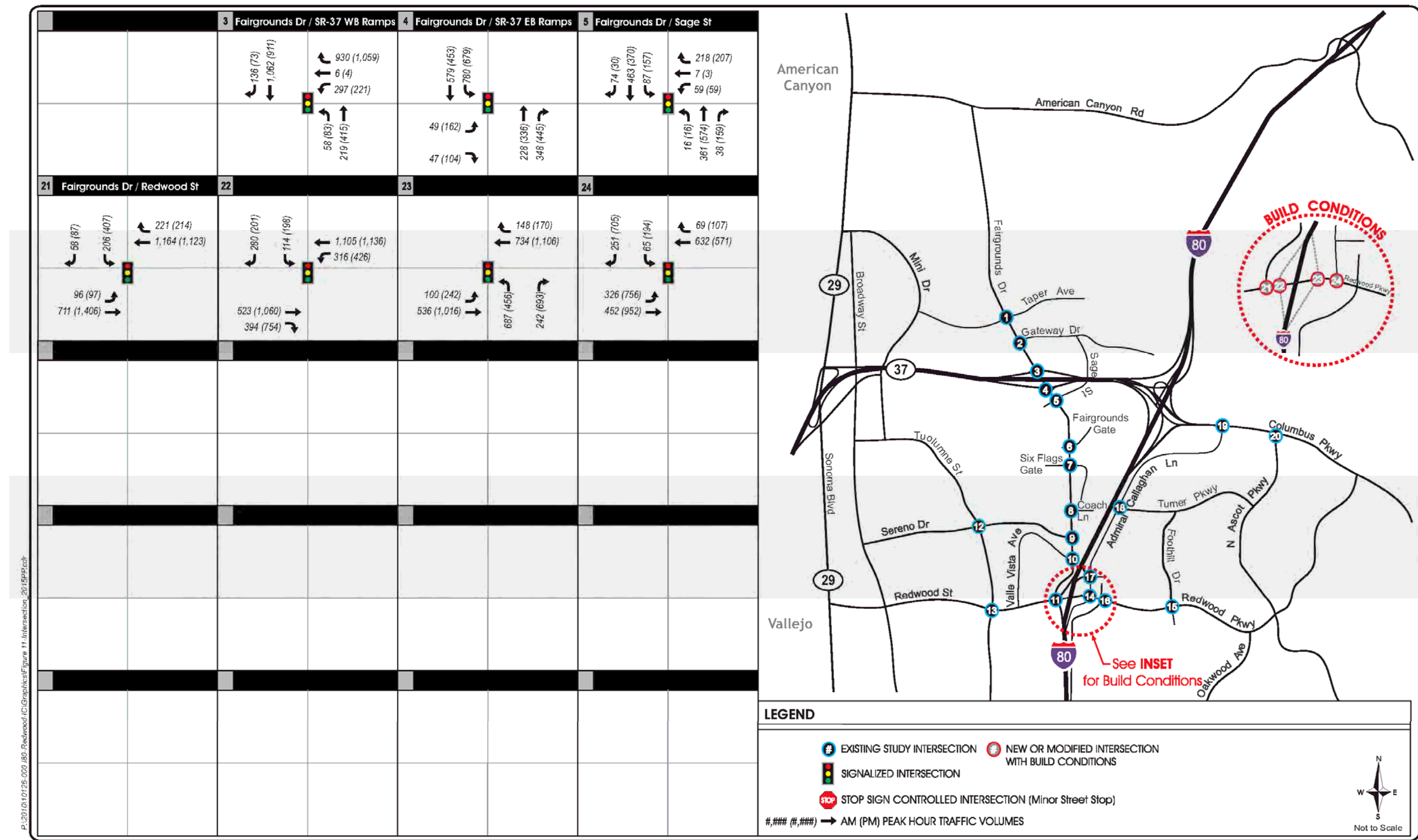


Figure 25 2015 build intersection volumes

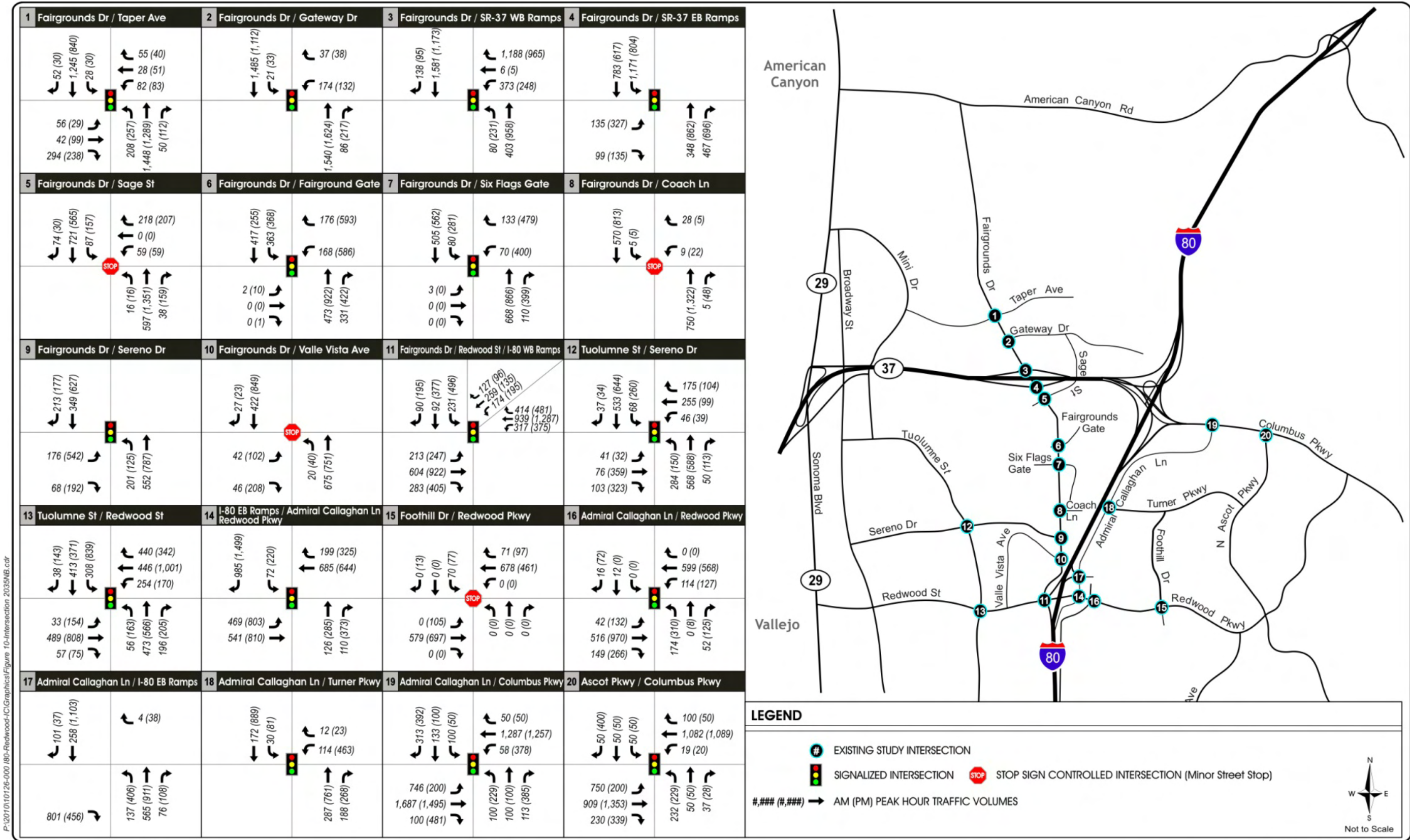


Figure 26 2035 no build intersection volumes

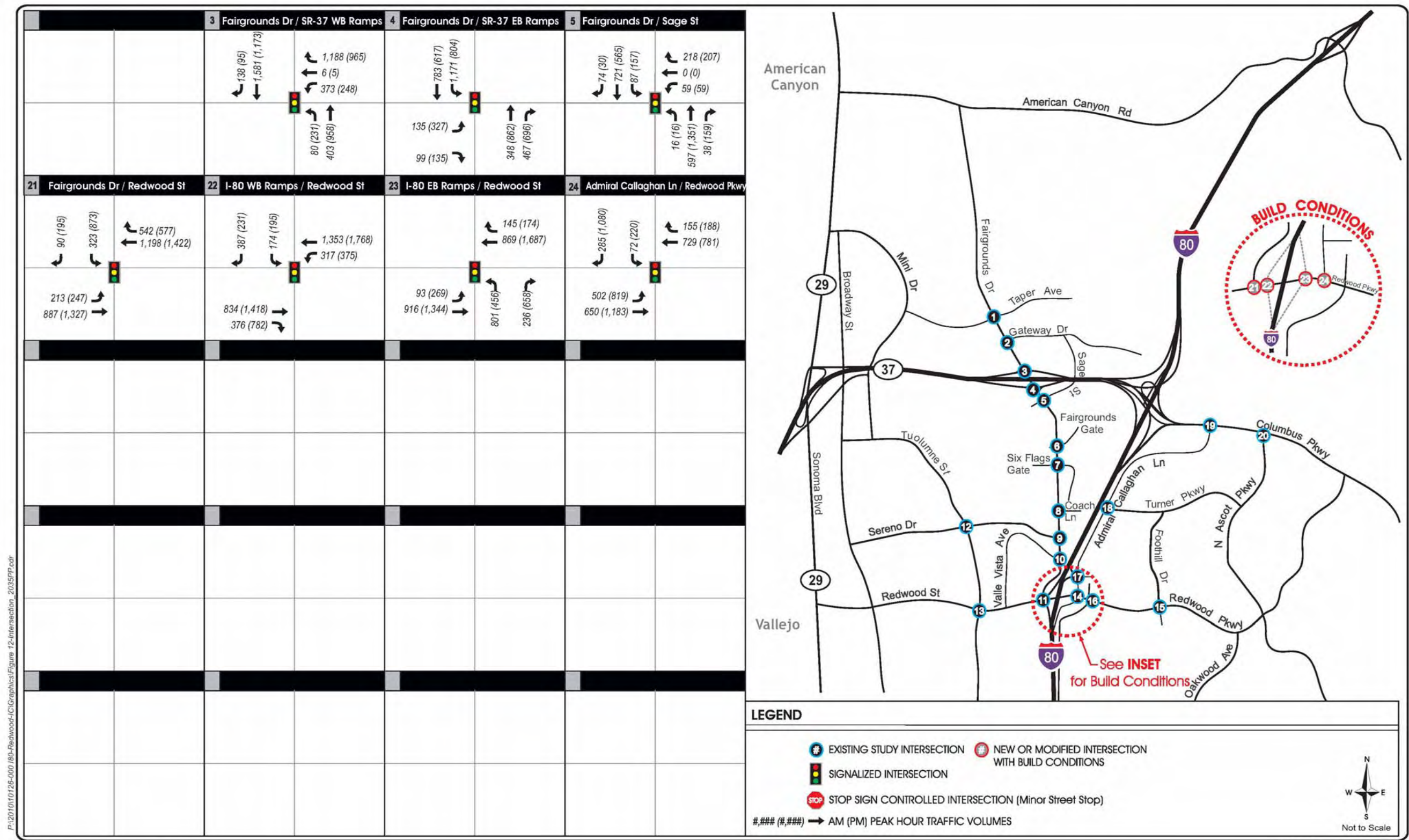


Figure 27 2035 build intersection volumes

Attachment F

Feasible Noise Barrier Locations

DATE	COUNTY	SECTION	PROJECT	SHEET NO.
04	SOI	80, 37	4-0/4-3	10-8/11-2

REGISTERED CIVIL ENGINEER DATE	PLANS APPROVAL DATE
10/8/11-2	4-0/4-3

NAME	ADDRESS	CITY	STATE	ZIP
1814 Franklin Street	Suite 700	Atlanta, GA	30309	30309

DATE	BY	REVISION
10/8/11-2	10/8/11-2	10/8/11-2

NOTES:

- FOR ACCURATE RIGHT OF WAY DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.
- EXISTING TREES NOT SHOWN ON ALL LAYOUT SHEETS.

LEGEND:

- BOLLARD
- ▤ OBSTRUCTED AC SURFACING
- DIRECTION OF TRAFFIC

DATE REVISION

CHECKED BY

DESIGNED BY

FUNCTIONAL SUPERVISOR

STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION

DATE REVISION

CHECKED BY

DESIGNED BY

FUNCTIONAL SUPERVISOR

DATE REVISION

CHECKED BY

DESIGNED BY

FUNCTIONAL SUPERVISOR

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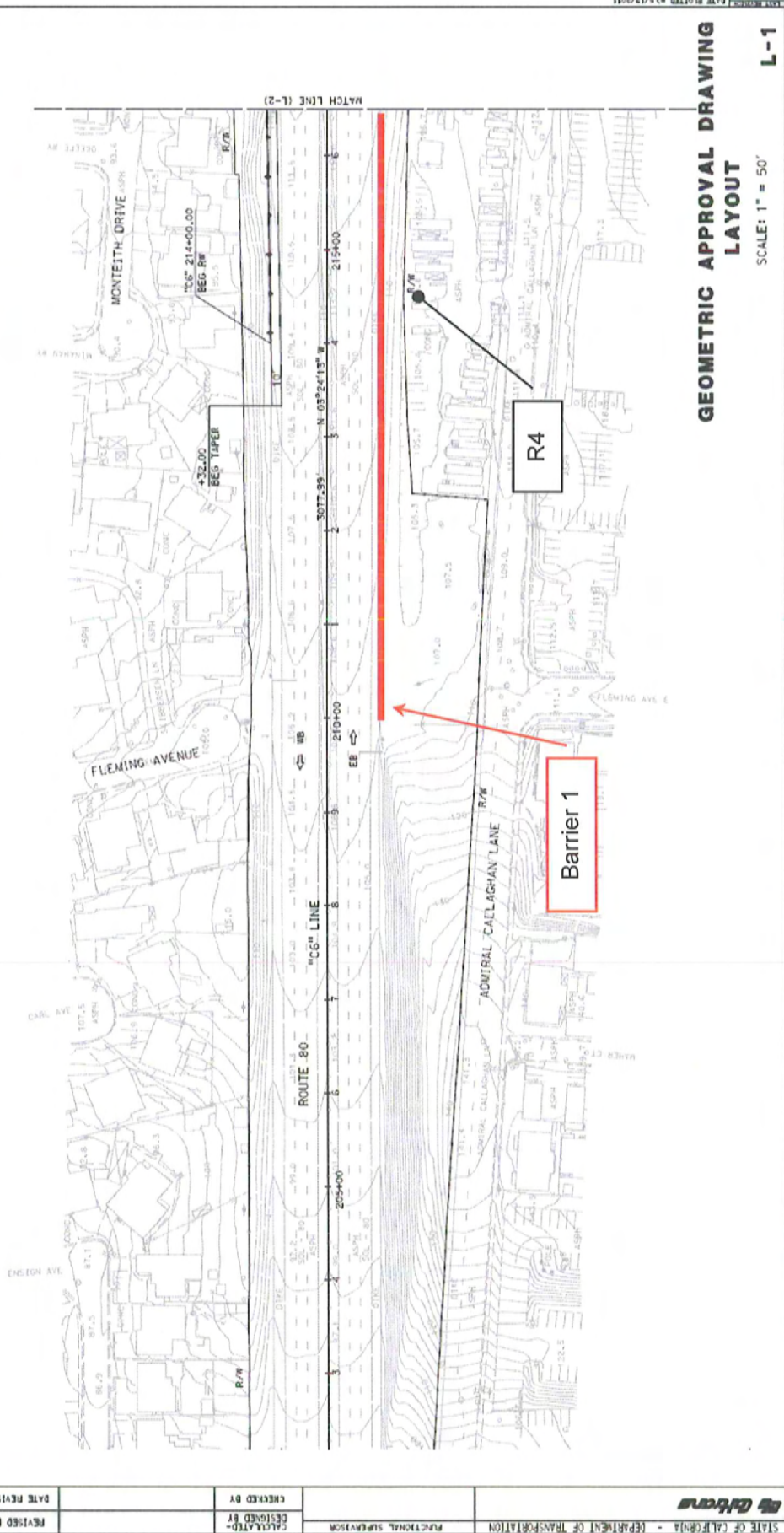
FUNCTIONAL SUPERVISOR

DATE REVISION

CHECKED BY

DESIGNED BY

FUNCTIONAL SUPERVISOR



STATE OF CALIFORNIA - DEPARTMENT OF TRANSPORTATION	FUNCTIONAL SUPERVISION	DESIGNED BY	CHECKED BY	DATE REVISOR	REVISOR

BORER LAST REVISED 3/1/2007

RELATIVE ELEVATION SCALE
1" = 10' INCHES

USNAME and state
DOE FILE no. ...\\usname\usname-2004.dwg

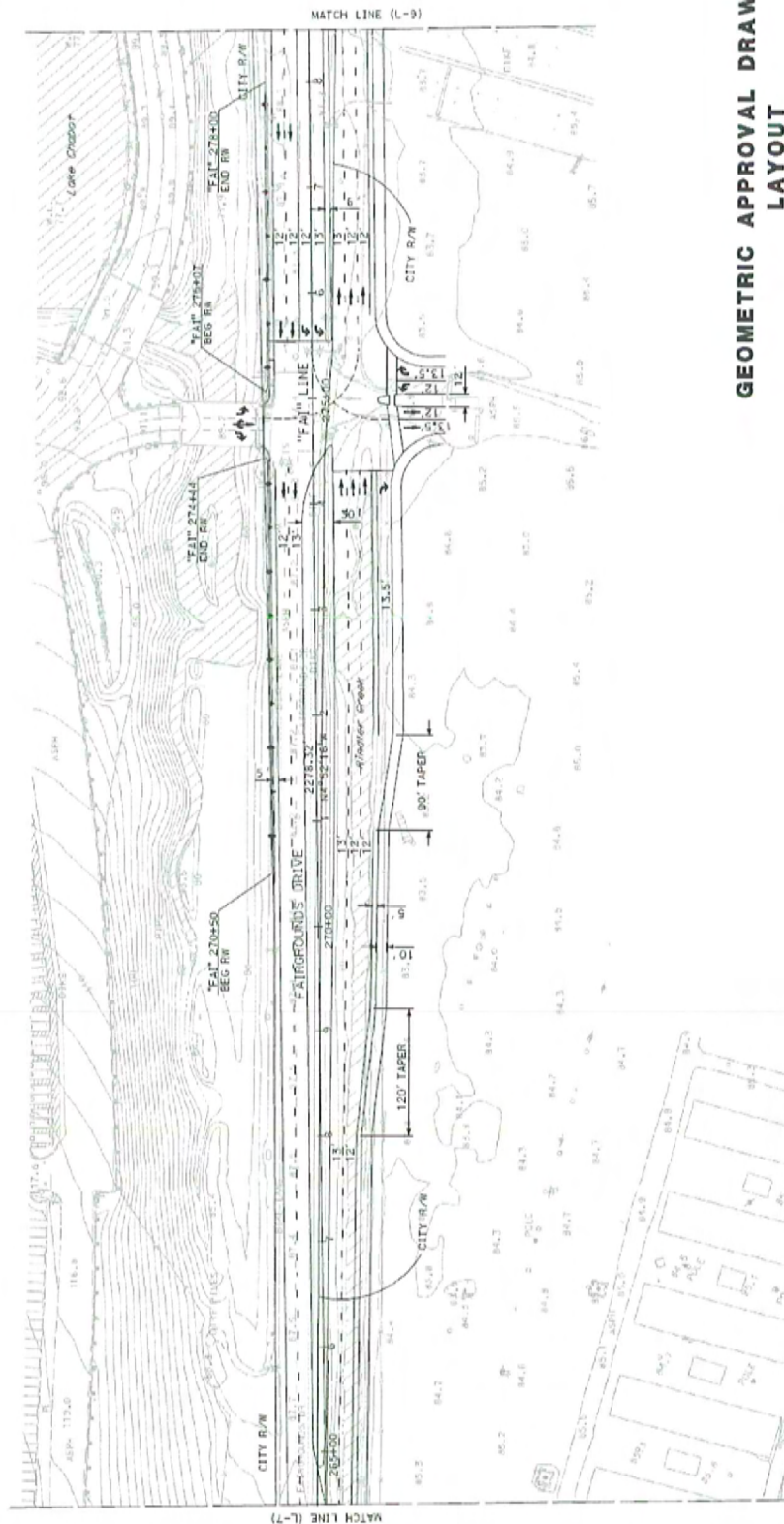
CU 00000

E4 444410

SCALE: 1" = 50'

L-8

GEOMETRIC APPROVAL DRAWING LAYOUT



CITY	COUNTY	LOCATION	DATE	SCALE	PROJECT
04	San	80, 37	4.0/4.9	10.6/11.3	
REGISTERED CIVIL ENGINEER DATE					
PROJECT APPROVAL DATE					
THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION					
1814 FORTKIN STREET					
SAN JOSE, CALIFORNIA 95126					
CONTRACT NO. 444410					

00-00-00-00
CITE PLATTER NO. 5/13/2011
11/10/2011

**GEOMETRIC APPROVAL DRAWING
LAYOUT**

8-7

SCALE: 1" = 50'



**Attachment G
Final EIR/EA and
Finding of No Significant Impact (FONSI)
Cover, Signature Page,
and Summary
(Complete FED Under Separate Cover)**

Redwood Parkway – Fairgrounds Drive Improvement Project

SOLANO COUNTY, CALIFORNIA

DISTRICT 04-Sol-80 PM 4.0/4.9

04-Sol-37 PM 10.6/11.2

EA 4A4410/Project No. 0400020584

SCH No. 2011012032

Final Environmental Impact Report/ Environmental Assessment (EIR/EA)



Prepared by the State of California Department of Transportation and
Solano Transportation Authority

The environmental review, consultation, and any other action required in accordance with applicable Federal laws for this project is being, or has been, carried out by Caltrans under its assumption of responsibility pursuant to 23 USC 327.



June 2015

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General Information About This Document

For individuals with sensory disabilities, this document can be made available in Braille, in large print, on audiocassette, or on computer disk. To obtain a copy in one of these alternate formats, please call or write to Department of Transportation, Attn: Zachary Gifford, 111 Grand Avenue, Office of Environmental Analysis, Oakland, CA, 94612; (510) 286-5610, Voice, or use the California Relay Service TTY number, 711. An electronic copy of the document can also be accessed at the following website http://www.dot.ca.gov/dist4/projects_list.htm.

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SCH No. 2011012032
DISTRICT 04-SOL-80-PM 4.0/4.9
04-Sol-37 PM10.6/11.2
EA 4A4410/Project No. 0400020584

Widen and improve Fairgrounds Drive from the Interstate 80/Redwood Parkway interchange (post mile 4.0/4.9) to the State Route 37/Fairgrounds Drive interchange (post mile 10.6/11.2), including interchange and intersection modifications.

FINAL ENVIRONMENTAL IMPACT REPORT/ENVIRONMENTAL ASSESSMENT

Submitted Pursuant to: (State) Division 13, California Public Resources Code
(Federal) 42 USC 4332(2)(C)

THE STATE OF CALIFORNIA
Department of Transportation

and

SOLANO TRANSPORTATION AUTHORITY

June 17, 2015
Date of Approval

Daniel J. McElhenny
Chief Deputy for Bijan Sartipi
District Director
Department of Transportation, District 4
NEPA Lead Agency

JUNE 2, 2015
Date of Approval

Daryl K. Halls
Daryl K. Halls
Executive Director
Solano Transportation Authority
CEQA Lead Agency

The following persons may be contacted for additional information concerning this document:

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Suisun City, CA 94585
(707) 424-6075

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CALIFORNIA DEPARTMENT OF TRANSPORTATION
FINDING OF NO SIGNIFICANT IMPACT (FONSI)

Redwood Parkway – Fairgrounds Drive Improvement Project
EA 4A4410/Project No.0400020584
Final EIR/EA

FOR

The California Department of Transportation (Caltrans) has determined that the Build Alternative will have no significant impact on the human environment. This FONSI is based on the attached Environmental Assessment (EA) for the Redwood Parkway – Fairgrounds Drive Improvement Project which has been independently evaluated by Caltrans and determined to adequately and accurately discuss the need, environmental issues, and impacts of the proposed project and appropriate mitigation measures. It provides sufficient evidence and analysis for determining that an Environmental Impact Statement is not required. Caltrans takes full responsibility for the accuracy, scope, and content of the attached EA.

The environmental review, consultation, and any other action required in accordance with applicable Federal laws for this project is being, or has been, carried-out by Caltrans under its assumption of responsibility pursuant to 23 USC 327.

Date

June 17, 2015

David A. McElhenny
Chief Deputy for Bijan Sartipi
Department District Director
Department of Transportation, District 4

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Summary

The Solano Transportation Authority (STA), Solano County, and the City of Vallejo, in cooperation with the California Department of Transportation (Department), propose to modify the existing Interstate 80 (I-80)/Redwood Parkway interchange to a tight diamond configuration, realign Fairgrounds Drive to a tee intersection north of the I-80 westbound ramps, widen Fairgrounds Drive between Redwood Street and State Route 37 (SR 37), widen the westbound exit ramp from SR 37 to Fairgrounds Drive, and improve the intersections at the SR 37/Fairgrounds Drive Interchange. Current transportation issues in this area include poor circulation during peak commute periods, long delays at intersections, short acceleration and deceleration areas, and limited sight distance. In addition, the existing capacity of the roadways in this area would not accommodate the projected future traffic volumes. **Figure 1-1** depicts the project location and **Figures 1-2a** through **1-2c** depicts the proposed Build Alternative improvements.

JOINT CEQA/NEPA DOCUMENT

The project is subject to Federal and State environmental review requirements because STA proposes the use of federal funds from the Federal Highway Administration (FHWA) and/or project requires an approval from FHWA. Project documentation, therefore, has been prepared in compliance with both the California Environmental Quality Act (CEQA) and the National Environmental Policy Act (NEPA). STA is the project proponent and the lead agency under CEQA. FHWA's responsibility for environmental review, consultation, and any other action required in accordance with applicable federal laws for this project is being, or has been, carried-out by Caltrans under its assumption of responsibility pursuant to Section 6005 of SAFETEA-LU codified at 23 United States Code (USC) 327(a)(2)(A). With NEPA Assignment, FHWA assigned and the Department assumed all of the United States Department of Transportation (USDOT) Secretary's responsibilities under NEPA. This assignment includes projects on the State Highway System and Local Assistance Projects off of the State Highway System within the State of California, except for certain categorical exclusions that FHWA assigned to the Department under the 23 USC 326 CE Assignment MOU, projects excluded by definition, and specific project exclusions.

Some impacts determined to be significant under CEQA may not lead to a determination of significance under NEPA. Because NEPA is concerned with the significance of the project as a whole, quite often a "lower level" document is prepared for NEPA. One of the most common joint document types is an Environmental Impact Report/Environmental Assessment (EIR/EA).

After receiving comments from the public and reviewing agencies, a Final EIR/EA was prepared. STA and the Department undertook additional environmental and/or engineering studies to address comments. The Final EIR/EA includes responses to comments received on the Draft EIR/EA and identifies the preferred alternative. If the decision is made to approve the project, a Notice of Determination will be published for compliance with CEQA, and the Department will decide whether to issue a Finding of No Significant Impact (FONSI) or require an Environmental Impact Statement (EIS) for compliance with NEPA. A Notice of Availability (NOA) of the FONSI will be sent to the affected units of federal, state, and local government, and to the State Clearinghouse in compliance with Executive Order 12372.

Any changes to the draft EIR/EA, as a result of comments received, are denoted with a vertical line in the right margin and referenced in **Chapter 4.0, Comments and Coordination**.

OVERVIEW OF THE PROJECT AREA

The proposed improvements are located within an existing urban context, with a mixture of commercial, office, residential, and recreation facility developments. Beginning at the southernmost portion of the project study area, the I-80/Redwood Parkway interchange and Redwood Parkway/Fairgrounds Drive intersection are surrounded by a mixture of commercial and residential development. The area along Fairgrounds Drive, between Valle Vista Avenue and Coach Lane is developed with multi-family homes and medical office buildings, as well as vacant lands. The area between Coach Lane and SR 37 along Fairgrounds Drive is primarily developed with recreational facilities. Six Flags Discovery Kingdom Amusement Park (Six Flags) and associated surface parking areas are located to west of Fairgrounds Drive. Lake Chabot is also on the west side of Fairgrounds Drive. The Solano County Fairgrounds and associated surface parking areas are located to the east, along with a Courtyard Marriot hotel and fast-food restaurants. The area to the north of SR 37, along Fairgrounds Drive, is comprised of single-family homes, a gas station, and Best Western Inn hotel.

Rindler Creek enters the project study area at the intersection of Coach Lane and Fairgrounds Drive, from under I-80 and then follows the outer boundary of the County Fairgrounds property. The creek flows northwest along Fairgrounds Drive before crossing beneath the road via a series of culverts. The creek forms some backwater channels between the road embankment and the embankment for the Six Flags Amusement Park, and then flows into Lake Chabot.

Related Projects

The revitalization of the 149-acre Solano County Fairgrounds property, located on the east side of Fairgrounds Drive, between Coach Lane and SR 37 is planned for future redevelopment. Future land uses include features such as a public entertainment zone and the fair of the future zone. The public entertainment zone would provide an active gathering place that would be home to a waterside pedestrian trail, restaurants, public art,

main street shops, terraced seating, and water-related activities. The fair zone continues the 60-year tradition of the annual Solano County Fair and would house a world class exhibition hall, organic demonstration farm, children's discovery island, and flexible sports fields and other multi-use facilities. The analyses of the potential effects of the proposed Build Alternative reflect the local land use and road improvements planned to be in place by 2035.

PURPOSE AND NEED

Current transportation issues within the project corridor include poor circulation during peak commute periods, long delays at intersections, short acceleration and deceleration areas, and limited sight distance. In addition, the existing capacity of the roadways in this area will not accommodate projected future traffic volumes planned for in the project vicinity.

The purpose of the project is to address these issues by:

- Relieving existing congestion and improving traffic flow on the local roadway network for approved redevelopment and planned land uses in the area;
- Improving the existing interchanges and intersection operations; and
- Improving the safety of the local roadway network by reducing congestion.

PROPOSED ACTION

The types of interchange improvements that would be possible at the existing Fairgrounds Drive/SR 37 and the Redwood Parkway/I-80 interchange are limited because these areas are physically constrained by the existing residential and commercial development. With the exception of the Build Alternative, other interchange configurations would require the reconstruction of the existing overcrossing structures and have severe right-of-way impacts combined with extremely high construction costs. Similarly, along the Fairgrounds Drive right-of-way, no other alignment alternatives were possible because of the steep grades and developed land uses and/or water features on either side of the roadway.

Because of these constraints, no other design alternatives were carried forward beyond initial design screenings. The alternatives evaluated in this environmental document include the Build Alternative and the No-Build (No Action) Alternative.

Build Alternative

Figures 1-2a through **1-2c** illustrate the improvements proposed under the Build Alternative, which would include the following major elements:

- Modification of the Redwood Parkway/I-80 Interchange
- Relocation of the Fairgrounds Drive/Redwood Street Intersection

- Moorland Street Cul-de-sacs
- Widening of Fairgrounds Drive
- Modifications to the Fairgrounds Drive/SR 37 interchange
- Signal Modifications

The total length of the project corridor is approximately 1.5 miles, and extends from the Fairgrounds Drive/ SR 37 interchange (postmile 4.0-4.9) to the Redwood Parkway/Interstate 80 (I-80) interchange (postmile 10.6-11.2).

No-Build (No Action) Alternative

The No-Build Alternative is being evaluated in accordance with NEPA and CEQA requirements, and serves as the baseline comparison to the Build Alternative. Under the No-Build Alternative, Fairgrounds Drive would maintain its existing configuration. No realignment of the Fairgrounds Drive/Redwood Street intersection would occur. There would be no improvements to the SR 37/Fairgrounds Drive or I-80/Redwood Parkway/Admiral Callaghan Lane interchanges.

Traffic volumes within the project corridor would increase under the No-Build Alternative. As there are no improvements proposed to the existing local roadway network, the No-Build Alternative would not achieve the project purpose of increasing the local roadway network capacity to accommodate existing and approved redevelopment and growth in the area. In addition, the increased traffic volumes without capacity improvements would worsen the congestion and slow traffic flow on the local roadway network. Without the realignment of the Fairgrounds Drive/Redwood Street intersection, the No-Build Alternative would not improve the current safety issues related to limited sight distance in this area. In addition, without modifying the I-80 eastbound ramps to a tight diamond configuration, short acceleration and deceleration lanes would remain, resulting in nonstandard merge and diverge distances.

Project Impacts

Table S-1 summarizes the adverse effects of the Build Alternative in comparison with the No-Build Alternative. The proposed avoidance, minimization, and/or mitigation measures to reduce the effects of the Build Alternative are also presented. For a complete description of potential adverse effects and recommended measures, please refer to the specific sections within **Chapter 2.0, Affected Environment, Environmental Consequences, and Avoidance, Minimization, and/or Mitigation Measures**.

Table S-1 Project Impacts

Environmental Topic	No Build Alternative	Build Alternative	Avoidance, Minimization, and/or Mitigation Measures
Air Quality			
Emissions from construction equipment	None expected	Temporary due to construction	Construction-related mitigation
Land Use			
Division of an established community	None expected	None expected	None
Compatibility with land use plans	Low	High consistency	None
Compatibility with habitat conservation plan	Not applicable	Not applicable	None
Growth			
No Effect			
Farmlands/Timberlands			
No Effect			
Community Impacts			
Displacement of existing housing/commercial and 17 commercial parcels	None	19 residential parcels potentially affected	Caltrans Relocation Assistance Program
Disproportionately affect environmental justice communities	No	No	None
Utilities/Emergency Services			
No Effect			
Traffic and Transportation			
Conflict with applicable plans, ordinances, policies, or programs	None	None	None
Increase congestion	Yes	Will reduce congestion	None

Environmental Topic	No Build Alternative	Build Alternative	Avoidance, Minimization, and/or Mitigation Measures
Increase hazards as a result of a design feature	None	None	None
Visual Resources			
Adverse effect on scenic views/damage scenic resources	Same as Build Alternative	No scenic resources in project area	None
Degradation of existing visual character or quality	None expected	Potential visual quality lost	Roadway design would adhere to City of Vallejo Standard Specifications All landscaping removed by project would be replaced
Create a new source of light or glare	None expected	Temporary due to construction	Caltrans light and glare screening measures
Cultural Resources			
Create an adverse change in the significance of a historical resource	None expected	No historical resources in project vicinity	None
Create an adverse change in the significance of an archaeological resource	None expected	No archaeological resources in project vicinity.	An Archaeological Monitoring and Discovery Plan has been prepared that specifies the appropriate construction monitoring locations and protocols recommended for an area near the known redeposit of archaeological materials outside of the project's area of potential effect (APE).
Disturbance to human remains	None expected	None expected	If human remains discovered, activity will stop (State Health and Safety Code Section 7050.5). If the remains are thought to be Native American, the Native American Heritage Commission will be contacted (Public Resources Code Section 5097.98).

Environmental Topic	No Build Alternative	Build Alternative	Avoidance, Minimization, and/or Mitigation Measures
Hydrology and Floodplain			
Within a 100-year floodplain	Same as Build Alternative	Small portion of Fairgrounds Drive, north of Coach Lane	None
Expose people/structures to a significant risk of loss	Unknown	None expected	None
Water Quality and Storm Water Runoff			
Result in substantial drainage pattern alteration	None	None expected	None
Violation of water quality standards	None	Temporarily during construction	Storm Water Pollution Prevention Plan
Change to groundwater supply or groundwater recharge	None	None	None
Substantially degrade water quality	None	Possible operation impacts	Design Pollution Prevention and Treatment Best Management BMPs.
Geology			
Expected Likelihood of seismic related issues, including ground shaking and liquefaction	Same as Build Alternative	High potential for ground shaking, liquefaction potential varies	Caltrans seismic design standards
Expose people or structures to potential adverse effects	None expected	Worker safety	Occupational Safety and Health Act Section 5(a)(1)
Paleontology			
Unearth previously unidentified paleontological resources (i.e., fossil remains and sites)	None expected	Potential due to excavation and construction activities	Preparation and implementation of a Department-approved paleontological monitoring and mitigation program. See Mitigation Measure PAL-1

Environmental Topic	No Build Alternative	Build Alternative	Avoidance, Minimization, and/or Mitigation Measures
Hazardous Materials			
Create a hazard to the environment	None expected	Potential due to excavation and construction activities	Additional subsurface sampling, Soil Management Plan, and Caltrans Variance Follow regulations requiring abatement of asbestos-containing materials and lead-based paint.
Create a hazard to the public	None expected	None expected	Additional subsurface sampling, Soil Management Plan, and Caltrans Variance Follow regulations requiring abatement of asbestos-containing materials and lead-based paint.
Location on a site that is included on a list of hazardous materials sites	Same as Build Alternative	Varies throughout project area, sites on several lists	Additional subsurface sampling, Soil Management Plan, and Caltrans Variance Follow regulations requiring abatement of asbestos-containing materials and lead-based paint.
Noise			
Exposure of the public to excessive noise levels, including groundborne noise levels	None	Some temporary noise effects, no permanent ambient noise increase with mitigation	Noise abatement measures, sound walls
A substantial increase in permanent noise levels	None expected	Potential permanent noise level increases ranging from 0 to 6 dBA (varies throughout project area)	Potential noise abatement measures
A substantial increase in temporary noise levels	None	Due to construction activities	Restricted construction hours, equipment mufflers, equipment placed away from sensitive receptors, "quiet" air compressors, no unnecessary idling, equipment must conform to Standard Specifications

Environmental Topic	No Build Alternative	Build Alternative	Avoidance, Minimization, and/or Mitigation Measures
Energy			
No Effect			
Biological Resources			
Effects to sensitive or special status species	None	Western pond turtle, potential effect to bird habitat	Limit construction zone, limit artificial lighting, dispose of food-related trash, no firearms on site, no pets on site, conduct nesting bird surveys prior to construction and butterfly survey, biological monitor present during Rindler Creek relocation
Effects to habitat or sensitive natural communities	None	Wetlands/riparian woodlands effected due to realignment of Rindler Creek	Compensatory mitigation for jurisdictional water features. See Mitigation Measure BIO-1.
Conflict with local policies/plans	None	None	None

COORDINATION WITH PUBLIC AND OTHER AGENCIES

Notice of Preparation and Scoping

“Scoping” is the process of determining the scope, focus, and content of an environmental document. The scoping process allows agencies and other interested parties to provide input on the proposed project, range of alternatives, topics being evaluated, environmental effects, methods of assessment, and mitigation measures being considered.

Scoping for this project included the use of several channels of communication, including the Notice of Preparation (NOP), mailers, internet, and newspaper ads. In addition, a public open house scoping meeting was held on January 26, 2011 to inform the public and agencies of the project and scoping process. The NOP was issued to the State Clearinghouse on January 11, 2011. A mailer, which provided information on the project and details of the scoping meeting, was distributed to approximately 2,000 stakeholders in the project vicinity. Stakeholders include property owners within 500 feet of the project, elected officials and public agencies, special interest organizations, and neighborhood groups. The list of stakeholders was developed with the aid of the City of Vallejo Planning Department, the Solano 360 project stakeholder list, and local parcel data. This information was also posted on January 11, 2011 to the STA website: www.sta.ca.gov. The project information on the website was available both in English and Spanish and provided project location maps.

An e-mail address (fairgroundsdiveproject@gmail.com) was created as an additional method for the public to comment on the Build Alternative.

A display advertisement announcing the scoping period and the public open house scoping meeting ran in the Vallejo Times-Herald and Cronicas (the local Spanish-language newspaper) on Tuesday, January 11, 2011.

There were eight written comments submitted at the January 26 scoping meeting. Two comment sheets were mailed to STA and six e-mails were received via fairgroundsdiveproject@gmail.com. One comment letter was received from the California Department of Fish and Game, one letter was received from the Governor’s Office of Planning and Research, and one comment letter was received from the California Transportation Commission. Key issues raised during the scoping period are addressed in **Chapter 2.0, Affected Environment, Environmental Consequences, and Avoidance, Minimization, and/or Mitigation Measures**, of this environmental document.

A public meeting was held on January 18, 2012 to provide information and answer questions about the Build Alternative. Invitation letters were sent to property owners whose residence or business may potentially be directly impacted by the project. Thirteen property owners and residents signed in at the meeting and one written comment was received.

The public review period of the draft EIR/EA started September 21, 2012 and ended November 5, 2012. A public meeting was also held on October 11, 2012 during the 45-day review period of the draft EIR/EA. The meeting was held from 7:00 to 8:00 p.m at Cooper Elementary School, located at 612 Del Mar Avenue in Vallejo, California. The primary purpose of the meeting was to provide information, answer questions, and receive comments on the draft EIR/EA for the project. The secondary purpose of the meeting was to present the findings of the noise abatement options evaluated at potential noise affected areas along the project corridor, and receive public comments regarding the potential barrier locations.

Twenty-nine attendees signed in at the meeting. The meeting format was an open house, where attendees could view exhibit boards illustrating the proposed Build Alternative improvements and submit verbal and written comments. Members of the project team were present to answer questions and provide project information. A Spanish translator was present to assist with Spanish translation.

A total of 16 written comment forms were received at the meeting. No verbal comments were submitted. The majority of the concerns raised by the attendees were regarding right-of-way acquisition of private property. Other issues raised included general support or dislike for the project, the placement of noise barriers, and traffic safety. Copies of the written comments received during the meeting are included in **Section 4.2.2, Responses to Comments**.

Necessary Permits and Approvals

Table S-2 identifies the permits/approvals that would be required for project construction.

Table S-2 Permits and Approvals

Agency	Permit/Approval	Status
United States Army Corps of Engineers	Section 404 Permit – Nationwide	Issued during the Final Design Phase
United States Fish and Wildlife Service	Concurrence with “no effect” determination	Issued during the Final Design Phase
California Department of Fish and Game ¹	1602 Agreement	Issued during the Final Design Phase
California Water Resources Board	NPDES Permit	Issued during the Final Design Phase
Regional Water Quality Control Board	Section 401 Certification	Issued during the Final Design Phase

Agency	Permit/Approval	Status
Metropolitan Transportation Commission (MTC) Air Quality Conformity Task Force/ Federal Highway Administration (FHWA)	Regional Air Quality Conformity ¹	MTC Determination December 17, 2014 FHWA Determination February 2, 2015
	Project-Level Air Quality Conformity	MTC Determination October 6, 2011 FHWA Concurrence May 21, 2015
State Historic Preservation Officer (SHPO)	Concurrence on Eligibility Determinations/Finding of No Historic Properties Affected	Concurrence issued March 1, 2012

¹ As of January 1, 2013, the California Department of Fish and Game (CDFG) changed its name to California Department of Fish and Wildlife (CDFW). Because the draft EIR/EA was published in September 2012, prior to the agency's name change, "CDFG" is referenced in relevant correspondence and discussion in order to maintain consistency with the project's administrative record.

Temporary construction easements may be required from the City of Vallejo and Solano County to accommodate work outside State-owned right-of-way.

Attachment H

TMP Data Sheet

TRANSPORTATION MANAGEMENT PLAN DATA SHEET

For Consultant TMP Projects

PROJECT MANAGER (Name)	(Calnet#)
Jason Mac	
PROJECT ENGINEER (Name)	(Calnet#)
Heidi Ouren, HQE Incorporated	
DIST-EA: 04-4A4410	
PROGRAM (HB1, HE11, etc.): 20.20.400.XXX	
PROJECT COMMON NAME	
Redwood Parkway-Fairgrounds Drive Improvements	
CO-RTE-PM (KP):	
SOL-80 PM 4.0/4.9; SOL-37 PM 10.6/11.2	
LEGAL DESCRIPTION:	
<p>DETAILED WORK DESCRIPTION:</p> <p>This project proposes to modify the I-80/Redwood Parkway Interchange in order to eliminate weaving areas and improve current and future traffic operations. The Preferred Alternative will consist of some or all of the following major features depending upon the outcome of the engineering and environmental studies:</p> <ul style="list-style-type: none"> • Construction of a tight diamond at I-80/Redwood Parkway Interchange utilizing the existing structure over I-80; • Widening of Fairground Drive from two to four lanes from Redwood Street to Coach Lane, and from four to five lanes from Coach Lane to Route 37; • Modifications to the Route 37/Fairgrounds Drive Interchange; • Signalized intersections at the Redwood Parkway/I-80 eastbound ramps, Redwood Street/I-80 WB ramps, and Redwood Street/Fairgrounds Drive; • Signalized intersections at Fairgrounds Drive/Solano County Fairgrounds Development Entrance (south); • Signal modifications at Fairgrounds Drive/Route 37 WB ramps, Fairgrounds Drive/Route 37 eastbound ramps, Fairgrounds Drive/Solano County Fairgrounds Development Entrance (north), Sereno Drive/Fairground Drive, and Redwood Parkway/Admiral Callaghan Way; • Relocation of the Fairgrounds Drive/Redwood Street intersection; • Cul-de-sac at Moorland Street west of Fairgrounds Drive; • Retaining walls; • Sound walls; and • Right of Way acquisition. 	
CONSTRUCTION COST ESTIMATE: \$49 M (Includes R/W)	
PROJECT PHASE:	PSR <input type="checkbox"/> PR <input checked="" type="checkbox"/> PS&E <input type="checkbox"/> %

Traffic Impact Description

- A) Does the proposed project includes long term closures (> 24 hours) Yes _____ No X
 [Check Applicable Facilities. If "No", Continue to Item D (Preliminary TMP Elements and Costs.)]
- ☐ Freeway Lanes
 - ☐ Freeway Shoulder
 - ☐ Freeway Connectors
 - ☐ Freeway Off-ramps

- ☐ Freeway On-ramps
- ☐ Local Streets
- ☐ Full Freeway Closures

B) Are there any construction strategies that can restore existing number of lanes?

(Check Applicable Strategies)

Yes _____ No _____

- ☐ Temporary Roadway Widening Structure Involvement?
If yes, notify Project Manager
- ☐ Lane Restriping (Temporary Narrow Lane Widths)
- ☐ Roadway Realignment (Detour Around Work Area)
- ☐ Median and/or Right Shoulder Utilization
- ☐ Use of HO V lane as a Temporary Mixed Flow Lane
- ☐ Staging Alternatives (Explain Below)

Yes _____ No _____

Notes:

C) Calculated Delays (To be performed if construction strategies in Item B do not mitigate congestion resulting from Item A)

1. Estimated Maximum Individual Vehicle Delay _____ Minutes
2. Existing or Acceptable Individual Vehicle Delay _____ Minutes
3. Estimated Individual Vehicle Delay Requiring Mitigation
[(1) - (2)] _____ Minutes
4. Estimated Delay Cost (Most Applicable)
 - ☐ Extended Weekend Closure \$ _____
 - ☐ Weekly (7 days) \$ _____
5. Estimated Duration of Project Related Delays \$ _____
6. Cost of Construction Related Delays [(4 x 5)] \$ _____

D) Preliminary TMP Elements and Cost

1) Public Information

- ☒ a. Brochures and Mailers \$15,000
- ☒ b. Press Release
- ☐ c. Paid Advertising \$
- ☐ d. Public Information Center/Kiosk \$
- ☐ e. Public Meeting/Speakers Bureau
- ☐ f. Telephone Hotline
- ☒ g. Internet, E-mail
- ☐ h. Notification to impacted groups
(i.e. bicycle users, pedestrians with disabilities, others...)
- ☐ i. Others \$

2) Motorist Information Strategies

<input type="checkbox"/> a. Changeable Message Signs (Fixed)	
<input checked="" type="checkbox"/> b. Changeable Message Signs (Portable)	\$25,000
<input checked="" type="checkbox"/> c. Ground Mounted Signs	\$50,000
<input type="checkbox"/> d. Highway Advisory Radio	\$
<input type="checkbox"/> e. Caltrans Highway Information Network (CHIN)	
<input type="checkbox"/> f. Detour maps (i.e. bicycle, vehicle, pedestrian...etc)	
<input type="checkbox"/> g. Revised Transit Schedules/maps	
<input checked="" type="checkbox"/> h. Bicycle community information	
<input checked="" type="checkbox"/> i. Others (Staging, Detours & Flagging)	\$100,000

3) Incident Management

<input checked="" type="checkbox"/> a. Construction Zone Enhanced Enforcement Program (COZEEP)	\$45,000
<input type="checkbox"/> b. Freeway Service Patrol	\$
<input type="checkbox"/> c. Traffic Management Team	
<input type="checkbox"/> d. Helicopter Surveillance	\$
<input type="checkbox"/> e. Traffic Surveillance Stations (Loop Detector and CCTV)	\$
<input type="checkbox"/> f. Others	\$

TMP Data Sheet (cont.)

4) Construction Strategies

<input checked="" type="checkbox"/> a. Lane Closure Chart	
<input type="checkbox"/> b. Reversible Lanes	
<input type="checkbox"/> c. Total Facility Closure	
<input type="checkbox"/> d. Contra Flow	
<input type="checkbox"/> e. Truck Traffic Restrictions	\$
<input checked="" type="checkbox"/> f. Reduced Speed Zone	\$
<input checked="" type="checkbox"/> g. Connector and Ramp Closures	
<input type="checkbox"/> h. Incentive and Disincentive	\$
<input type="checkbox"/> i. Moveable Barrier	\$
<input type="checkbox"/> j. Others	\$

5) Demand Management

<input type="checkbox"/> a. HOV Lanes/Ramps (New or Convert)	\$
<input type="checkbox"/> b. Park and Ride Lots	\$
<input type="checkbox"/> c. Rideshare Incentives	\$
<input type="checkbox"/> d. Variable Work Hours	
<input type="checkbox"/> e. Telecommute	
<input type="checkbox"/> f. Ramp Metering (Temporary Installation)	\$

<input type="checkbox"/> g. Ramp Metering (Modify Existing)	\$ _____
<input type="checkbox"/> h. Others _____	\$ _____
6) Alternative Route Strategies	
<input type="checkbox"/> a. Add Capacity to Freeway Connector	\$ _____
<input type="checkbox"/> b. Street Improvement (widening, traffic signal... etc)	\$ _____
<input type="checkbox"/> c. Traffic Control Officers	\$ _____
<input type="checkbox"/> d. Parking Restrictions	_____
<input type="checkbox"/> e. Others _____	\$ _____
7) Other Strategies	
<input type="checkbox"/> a. Application of New Technology	\$ _____
<input type="checkbox"/> e. Others _____	\$ _____

TOTAL ESTIMATED COST OF TMP ELEMENTS = \$235,000

8. The Project includes the following: (Check applicable type of facility closures)

- X a. Highway or Freeway Lanes
- X b. Highway or Freeway Shoulders
- ☐ c. Full Freeway Closure
- X d. Freeway On/Off-Ramps
- X e. Freeway Connectors
- X f. Local Streets

9. Major operations requiring traffic control and working days for each

<u>Operation</u>	<u># of working days</u>
<input type="checkbox"/> a. Clearing and Grubbing	_____
<input type="checkbox"/> b. Existing Feature Removal	_____
<input type="checkbox"/> c. Excavation of Embankments Construction	_____
<input type="checkbox"/> d. Structural Section Construction	_____
<input type="checkbox"/> e. Drainage Feature Construction	_____
<input type="checkbox"/> f. Structures Construction	_____
<input type="checkbox"/> g. MBGR/Barrier Construction	_____
<input type="checkbox"/> h. Striping	_____
<input type="checkbox"/> i. Electrical Component Construction	_____
<input type="checkbox"/> j. Other	_____

Total days requiring Traffic Control TBD During PS&E

TOTAL ESTIMATED COST OF TMP ELEMENTS = \$235,000

Notes: Freeway lane and shoulder, and ramp closures are short term night closures during final conform of widened ramps. Local street closures would also be short term night closures during traffic switches..

PREPARED BY (Consultant)

CALTRANS OVERSIGHT ENG.

DATE

3/19/15

APPROVAL RECOMMENDED BY

(TMP Reviewer)

DATE

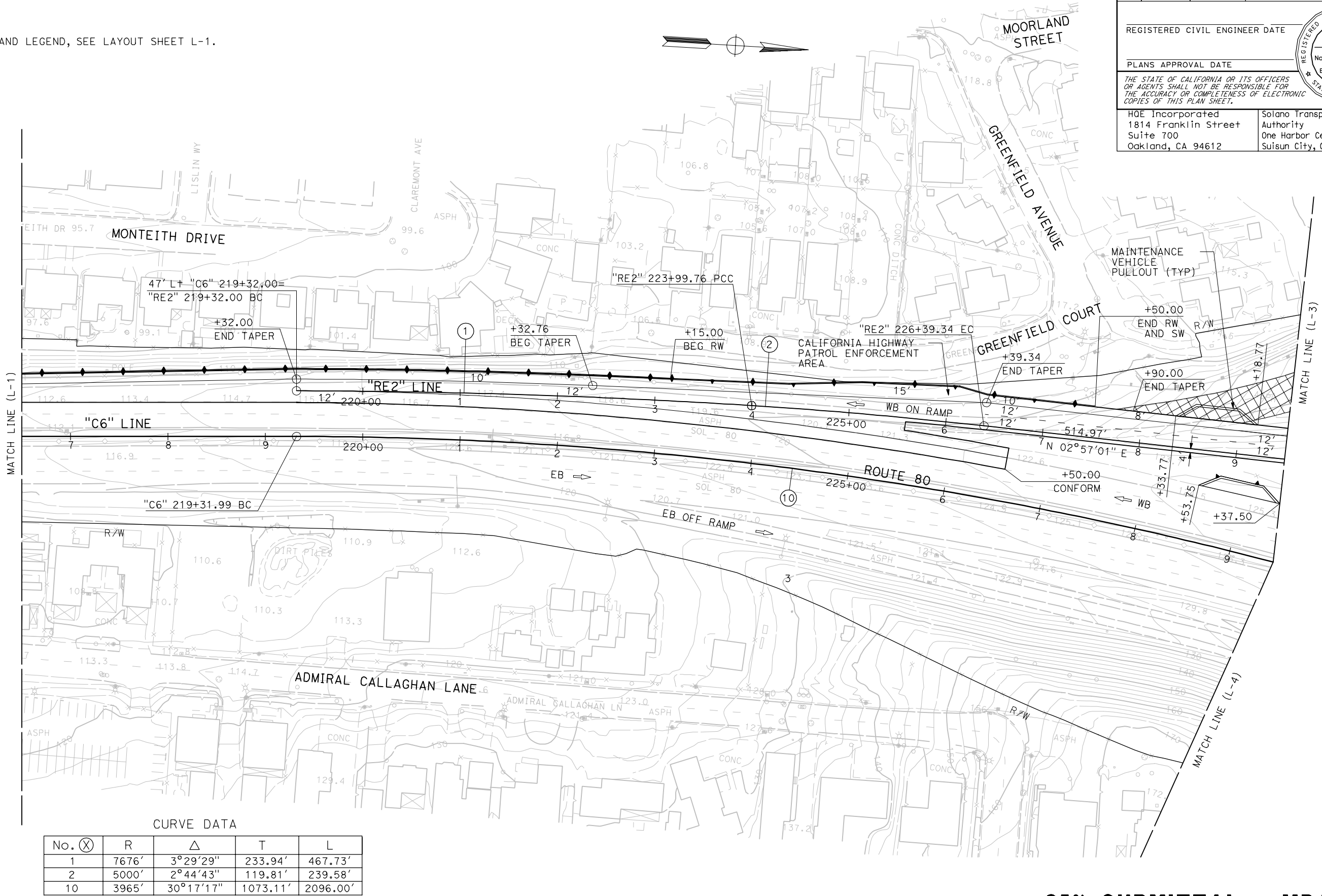
APPROVED BY (TMP Office)

DATE

Attachment I

Minimum Project Alternative Layouts

NOTE:
FOR NOTES AND LEGEND, SEE LAYOUT SHEET L-1.



CURVE DATA				
No. (X)	R	Δ	T	L
1	7676'	3°29'29"	233.94'	467.73'
2	5000'	2°44'43"	119.81'	239.58'
10	3965'	30°17'17"	1073.11'	2096.00'

35% SUBMITTAL - MPA
LAYOUT

SCALE: 1" = 50'

L-2

Dist	COUNTY	LOCATION CODE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
04	Soi	37, 80	4.0/4.9, 10.6/11.2		

REGISTERED CIVIL ENGINEER DATE

PLANS APPROVAL DATE

THE STATE OF CALIFORNIA OR ITS OFFICERS
OR AGENTS SHALL NOT BE RESPONSIBLE FOR
THE ACCURACY OR COMPLETENESS OF ELECTRONIC
COPIES OF THIS PLAN SHEET.

HQE Incorporated
1814 Franklin Street
Suite 700
Oakland, CA 94612

Solano Transportation
Authority
One Harbor Center, Suite 130
Suisun City, CA 94585

REGISTERED PROFESSIONAL ENGINEER
No. _____
Exp. _____
CIVIL
STATE OF CALIFORNIA

Dist	COUNTY	LOCATION CODE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
04	Soi	37, 80	4.0/4.9, 10.6/11.2		

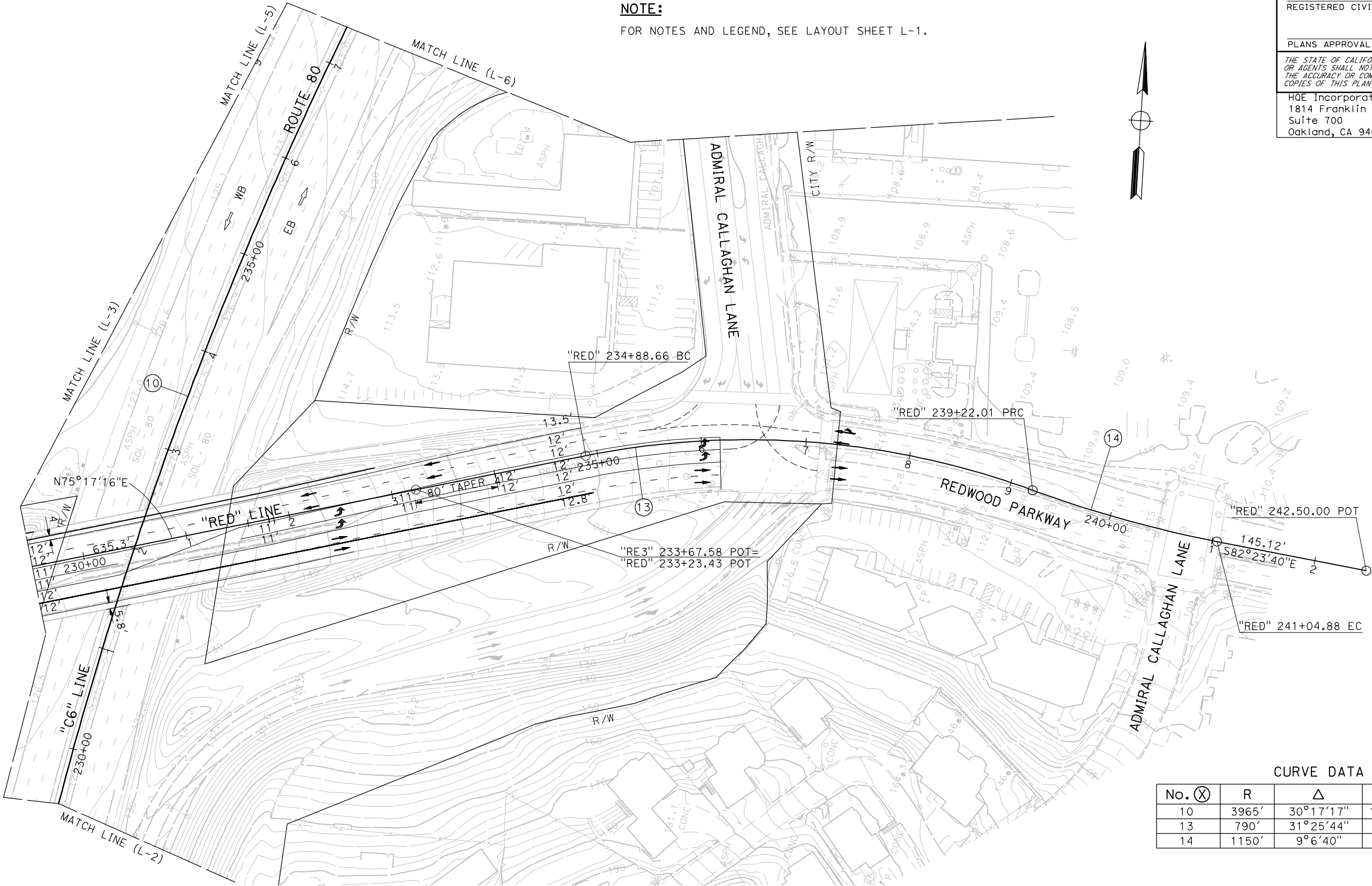
REGISTERED CIVIL ENGINEER DATE

PLANS APPROVAL DATE

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Oakland, CA 94612

Solano Transportation
Authority
One Harbor Center, Suite 130
Suisun City, CA 94585



CURVE DATA				
No. ⑩	R	Δ	T	L
10	3965'	30°17'17"	1073.11'	2096.00'
13	790'	31°25'44"	222.27'	433.35'
14	1150'	9°6'40"	91.63'	182.87'

35% SUBMITTAL - MPA

LAYOUT

SCALE: 1" = 50'

NOTE:
FOR NOTES AND LEGEND, SEE LAYOUT SHEET L-1.

Dist	COUNTY	LOCATION CODE	POST MILES TOTAL PROJECT	SHEET No.	TOTAL SHEETS
04	Sol	37, 80	4.0/4.9, 10.6/11.2		

REGISTERED CIVIL ENGINEER DATE

PLANS APPROVAL DATE

THE STATE OF CALIFORNIA OR ITS OFFICERS
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HQE Incorporated
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Oakland, CA 94612

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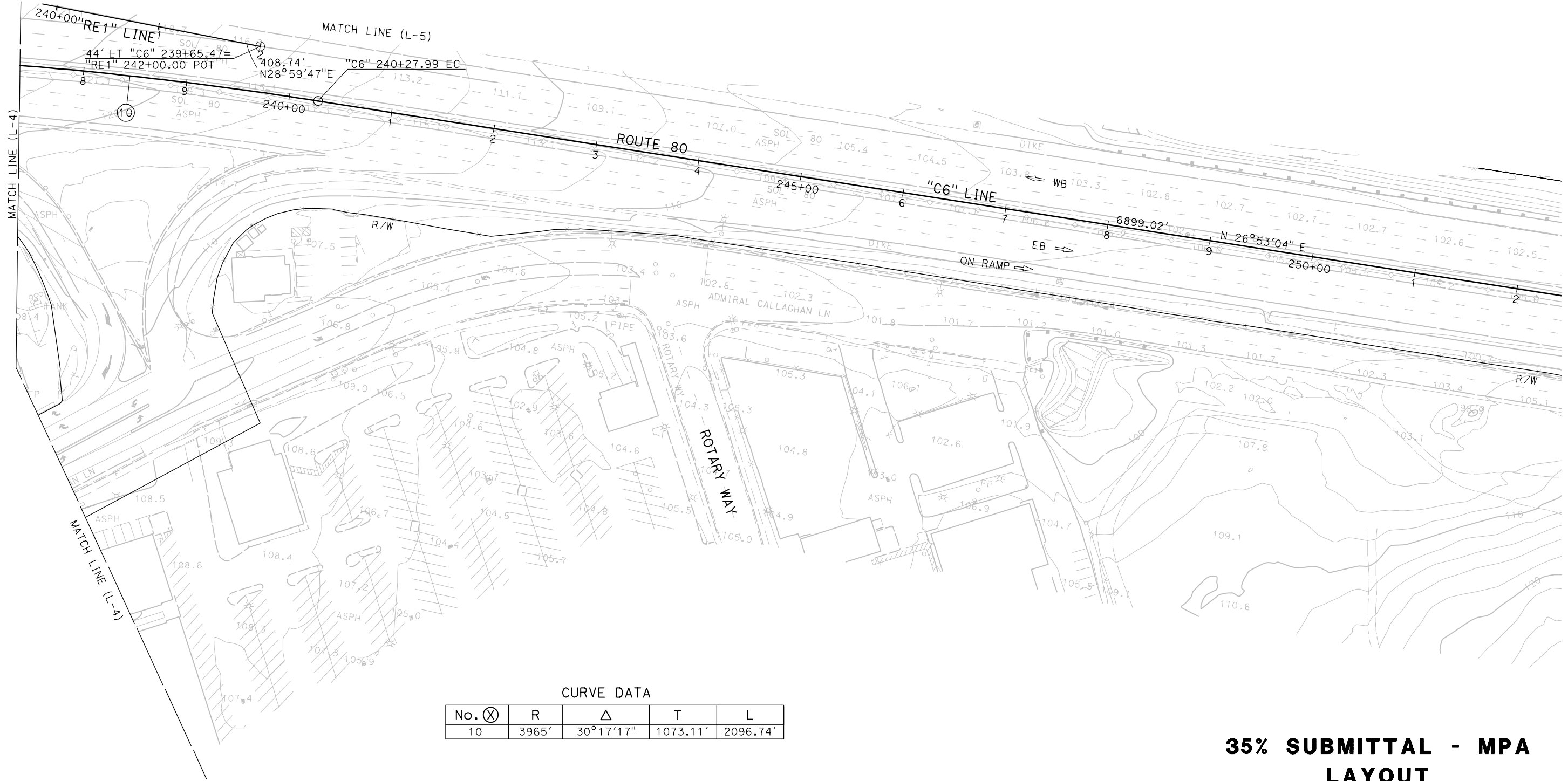
REGISTERED PROFESSIONAL ENGINEER

No. _____

Exp. _____

CIVIL

STATE OF CALIFORNIA



CURVE DATA				
No. (X)	R	Δ	T	L
10	3965'	30°17'17"	1073.11'	2096.74'

35% SUBMITTAL - MPA
LAYOUT

SCALE: 1" = 50'

Attachment J
Minimum Project Alternative
Cost Estimate

**PROJECT REPORT
PRELIMINARY COST ESTIMATE**

District-County-Route	04-SOL-80,04-SOL-37
PM	4.0/4.9, 10.6/11.2
EA	4A4410

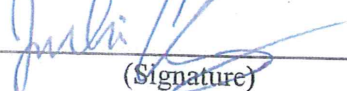
PROJECT DESCRIPTION:

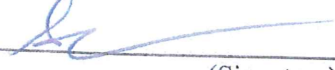
Limits Route 80/Redwood Parkway to Route 37
in the City of Vallejo

Proposed Improvement (Scope) Redwood Parkway Interchange Modifications - MPA
Fairground widening from Redwood Interchange to Route 37
and Route 37/Fairgrounds Drive Interchange improvements

SUMMARY OF PROJECT COST ESTIMATE

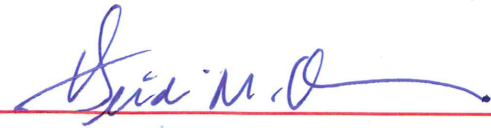
TOTAL ROADWAY ITEMS	\$	23,950,000
TOTAL STRUCTURE ITEMS	\$	-
SUBTOTAL CONSTRUCTION COSTS	\$	23,950,000
TOTAL RIGHT OF WAY ITEMS	\$	14,275,000
TOTAL PROJECT CAPITAL OUTLAY COSTS (Costs in 2012 Dollars)	\$	38,225,000

Review by 
(Signature)

Approved by  Date 8-17-12
(Signature)

Phone No. 810-763-4895

Note: Unit Costs were re-evaluated on January 28, 2015 to provide updated cost estimates. The results of the update indicate a reduction of 6.2%. Therefore, it is recommended that the unit costs applied in January 2012 for the Draft Project Report remain as is for the Final Project Report.


Heidi M. Ouren, P.E.
Project Manager

5/21/15

**PROJECT REPORT
PRELIMINARY COST ESTIMATE**

District-County-Route	04-SOL-80,04-SOL-37
PM	4.0/4.9, 10.6/11.2
EA	4A4410

I. ROADWAY ITEMS

<u>Section 1 Earthwork</u>	<u>Quantity</u>	<u>Unit</u>	<u>Unit Price</u>	<u>Item Cost</u>	<u>Section Cost</u>
Roadway Excavation	69000	CY	\$20.00	\$1,380,000	
Roadway Excavation(Y-1)		CY	\$33.00	\$0	
Imported Borrow		CY		\$0	
Clearing & Grubbing		LS		\$0	
Develop Water Supply		LS		\$0	
Cold Plane AC	38100	SY	\$1.50	\$60,000	
Subtotal Earthwork					\$1,440,000

Section 2 Pavement Structural Section*

Cement Treated Base	2000	CY	\$90.00	\$180,000	
ATPB	400	CY	\$180.00	\$70,000	
Class 4 Aggregate Subbase	5000	CY	\$55.00	\$280,000	
Class 2 Aggregate Base	11000	CY	\$60.00	\$660,000	
Asphalt Concrete (Type A)	700	TON	\$200.00	\$140,000	
Asphalt Concrete (Type B)	18600	TON	\$125.00	\$2,330,000	
OGAC	500	TON	\$140.00	\$70,000	
RAC-G	700	TON	\$150.00	\$110,000	
CL 1 PERM MTL	16000	CY	\$50.00	\$800,000	
Subtotal Pavement Structural Section					\$4,640,000

Section 3 Drainage

Large Drainage Facilities	1	LS	\$236,250.00	\$240,000	
Storm Drains	1	LS	\$1,148,000.00	\$1,150,000	
Subtotal Drainage					\$1,390,000

*Reference sketch showing typical pavement structural section elements of the roadway. Include (if available) T.I., R-Value and date when tests were performed.

NOTE: Extra lines are provided for items not listed, use additional lines as appropriate.

**PROJECT REPORT
PRELIMINARY COST ESTIMATE**

District-County-Route	04-SOL-80,04-SOL-37
PM	4.0/4.9, 10.6/11.2
EA	4A4410

<u>Section 4 Specialty Items</u>	<u>Quantity</u>	<u>Unit</u>	<u>Unit Price</u>	<u>Item Cost</u>	<u>Section Cost</u>
Retaining Walls	32238	SF	\$105.00	\$3,380,000	
Noise Barriers	1	LS	\$660,010.00	\$660,000	
Concrete Barrier (Type 60)	980	LF	\$65.00	\$60,000	
Concrete Barrier (Type 60C)		LF		\$0	
Highway Planting				\$0	
Replacement Planting	1	LS	\$345,000.00	\$350,000	
Irrigation Modification	1	LS	\$115,000.00	\$120,000	
Relocate Private Irrigation Facilities				\$0	
Erosion Control	1	LS	\$115,000.00	\$120,000	
Slope Protection				\$0	
Construction Site BMP	1	LS	\$345,000.00	\$350,000	
Hazardous Waste Mitigation		LS		\$0	
Work - See Roadway Ex. (Type Y)					
Environmental Mitigation	0.12	Acre	\$500,000.00	\$60,000	
Resident Engineer Office Space				\$0	
Prepare SWPPP & WPCP				\$0	
Treatment BMP	1	LS	\$574,000.00	\$570,000	
Landscaping/Irrigation (normally separate project)				\$0	
Temporary Railing (Type K)	14700	LF	\$14.00	\$210,000	
Subtotal Specialty Items					\$5,880,000

<u>Section 5 Traffic Items</u>					
Lighting		LS		\$0	
Traffic Delineation Items				\$0	
Traffic Signals	3	EA	\$200,000	\$600,000	
Traffic Signals (Mods)	2	EA	\$100,000	\$200,000	
Overhead Sign Structures	1	EA	\$100,000	\$100,000	
Roadside Signs				\$0	
Traffic Control Systems (Temporary)	1	LS	\$928,000.00	\$930,000	
TMP	1	LS	\$220,000.00	\$220,000	
Transportation Management (Permanent)		LS		\$0	
Ramp Metering Systems	1	EA	\$150,000	\$150,000	

Subtotal Traffic Items	\$2,200,000
------------------------	-------------

TOTAL SECTIONS 1 thru 5	\$15,550,000
-------------------------	--------------

NOTE: Extra lines are provided for items not listed, use additional lines as appropriate.

**PROJECT REPORT
PRELIMINARY COST ESTIMATE**

District-County-Route	04-SOL-80,04-SOL-37
PM	4.0/4.9, 10.6/11.2
EA	4A4410

Section 6 Minor Items

<u>\$15,550,000.00</u>	x	(10%)	=	<u>\$1,555,000</u>
(Subtotal Section 1 thru 5)				

TOTAL MINOR ITEMS	<u>\$1,555,000</u>
-------------------	--------------------

Section 7 Roadway Mobilization

<u>\$17,105,000.00</u>	x	(10%)	=	<u>\$1,710,000</u>
(Subtotal Section 1 thru 6)				

TOTAL ROADWAY MOBILIZATION	<u>\$1,710,000</u>
----------------------------	--------------------

Section 8 Roadway Additions

Supplemental Work

<u>\$17,105,000.00</u>	x	(10%)	=	<u>\$1,710,000</u>
(Subtotal Section 1 thru 6)				

Contingencies

<u>\$17,105,000.00</u>	x	(20%)	=	<u>\$3,420,000</u>
(Subtotal Section 1 thru 6)				

TOTAL ROADWAY ADDITIONS	<u>\$5,130,000</u>
-------------------------	--------------------

TOTAL ROADWAY ITEMS (Subtotal Section 1 thru 8)	<u>\$23,950,000</u>
--	---------------------

Estimate Prepared By	Jeff Leung (Print Name)	Phone#	510-763-4895	Date	1/13/2012
----------------------	----------------------------	--------	--------------	------	-----------

Estimate Checked By	Julia Chuang (Print Name)	Phone#	510-763-4895	Date	1/13/2012
---------------------	------------------------------	--------	--------------	------	-----------

** Use appropriate percentage per Chapter 20.

**PROJECT REPORT
PRELIMINARY COST ESTIMATE**

District-County-Route	<u>04-SOL-80,04-SOL-37</u>
PM	<u>4.0/4.9, 10.6/11.2</u>
EA	<u>4A4410</u>

II. STRUCTURES ITEMS

	Structure (1)	Structure (2)	Structure (3)	
Bridge Name	<u> </u>	<u> </u>	<u> </u>	
Structure Type	<u> </u>	<u> </u>	<u> </u>	
Width (out to out) - (ft)	<u> </u>	<u> </u>	<u> </u>	
Span Lengths - (ft)	<u> </u>	<u> </u>	<u> </u>	
Total Area - (sf)	<u> </u>	<u> </u>	<u> </u>	
Footing Type (pile/spread)	<u> </u>	<u> </u>	<u> </u>	<u> </u>
Cost Per SF	<u> </u>	<u> </u>	<u> </u>	<u> </u>
(incl. 10% mobilization and 25% contingency)				
Total Cost for Structure	<u> </u>	<u> </u>	<u> </u>	
SUBTOTAL STRUCTURAL ITEMS (Sum of Total Cost for Structure)				<u>\$0</u>
Railroad Related Costs:	<u> </u>			<u> </u>
	<u> </u>			<u> </u>
	<u> </u>			<u> </u>
TOTAL STRUCTURES ITEMS (Sum of Structures Items plus Railroad Items)				<u>\$0</u>

COMMENTS:

Estimate Prepared By	<u>Julia Chuang</u>	Phone#	<u>510-763-4895</u>	Date	<u>1/13/2012</u>
	(Print Name)				

NOTE: If appropriate, attach additional pages and backup.

PROJECT REPORT

PRELIMINARY COST ESTIMATE

District-County-Route	04-SOL-80,04-SOL-37
PM	4.0/4.9, 10.6/11.2
EA	4A4410

III. RIGHT OF WAY ITEMS

ESCALATED VALUE

A. Acquisition, including excess lands, damages to remainder(s) and Goodwill	<u>\$11,214,000</u>
B. Utility Relocation (State share)	<u>\$814,000</u>
C. Relocation Assistance	<u>\$990,000</u>
D. Clearance/Demolition	<u>\$1,257,000</u>
E. Title and Escrow Fees	(included in A. above)

TOTAL RIGHT OF WAY ITEMS	\$14,275,000
(Escalated Value)	

Anticipated Date of Right of Way Certification _____
(Date to which Values are Escalated)

F. Construction Contract Work

Brief Description of Work:

Right of Way Branch Cost Estimate for Work *

* This dollar amount is to be included in the Roadway and/or Structures Items of Work, as appropriate. Do not include in Right of Way Items.

COMMENTS:

Estimate Prepared By Julia Chuang Phone# 510-763-4895 Date 1/13/2012
(Print Name)

NOTE: If appropriate, attach additional pages and backup.

Attachment K
Minimum Project Alternative
Right of Way Data Sheet

RIGHT OF WAY DATA SHEET FOR LOCAL PUBLIC AGENCIES**Minimum Project Alternative** (Form #)

To: District Office Chief
Division of Right of Way and Land Surveys

Date: 01/11/2012

Co. SOL Rte. 80 P. M. 4.0/4.9
Rte. 37 P. M. 10.6/11.2

Attention: District Branch Chief
R/W Local Programs

Expense Authorization 4A4410

Subject: **RIGHT OF WAY DATA SHEET- LOCAL PUBLIC AGENCY SERVICES**

Project Description: I-80/ Redwood Parkway – Fairgoumds Drive Improvements – Minimum Project Alternative

Right of way necessary for the subject project will be the responsibility of Solano Transportation Authority.

The information in this data sheet was developed by HQE Incorporated and reviewed by Contra Costa County.

I. **Right of Way Engineering**

Will right of way engineering be required for this project?

- No
- Yes X (Submit a copy of the *Right of Way Engineering, Surveys and Mapping Services checklist for Special Funded Projects*. This checklist includes but is not limited to the following items.)

- | | |
|-------------------------------|-------------------|
| • Hard copy (base map) | <u> </u> Yes |
| • Appraisal map | <u> </u> Yes |
| • Acquisition Documents | <u> </u> Yes |
| • Property Transfer Documents | <u> </u> Yes |
| • R/W Record Map | <u> </u> Yes |
| • Record of Survey | <u> </u> Yes |

Remark: All the above documents will be provided at PS&E phase.

II. **Engineering Surveys**

1. Is any surveying or photogrammetric mapping required?

No Yes X (Complete the following)

Photogrammetric Mapping in accordance with CT requirements for Consultant Photogrammetric Mapping has already been prepared and approved. Additional site surveys are anticipated during PS&E.

2. **Datum Requirements**

Yes X Project will adhere to the following criteria:

- Horizontal - datum policy is NAD 83, CA-HPGN, EPOCH 1991.35 and English system of units.
- Vertical - datum policy is NAVD 88.
- Units - metric is not required.

No Provide an explanation on additional page.

3. Will land survey monument perpetuation be scoped into the project, if required?

Yes X

No Provide explanation on additional page.

No monument perpetuation required

RIGHT OF WAY DATA SHEET FOR LOCAL PUBLIC AGENCIES (Cont.)**Minimum Project Alternative** (Form #)R/W Data Sheet – Local Public Agencies
Page 2 of 5**III. Parcel Information (Land and Improvements)**

Are there any property rights required within the proposed project limits?

No _____ Yes X (Complete the following.)

	Part Take	Full Take	Estimate \$
A. Number of Vacant Land Parcels	<u>5</u>	_____	\$ <u>60,000</u>
B. Number of Single Family Residential Units	<u>5</u>	<u>13</u>	\$ <u>3,280,000</u>
C. Number of Multi-Family Residential Units	_____	<u>1</u>	\$ <u>400,000</u>
D. Number of Commercial/Industrial Parcels	<u>4</u>	<u>6</u>	\$ <u>7,474,000</u>
E. Number of Farm/Agricultural Parcels	_____	_____	\$ _____
F. Permanent and/or Temporary Easements	_____	_____	\$ _____
G. Other Parcels (define in "Remarks" section)	_____	_____	\$ _____
Totals	<u>14</u>	<u>20</u>	\$ <u>11,214,000</u>

The majority of the properties required for the project improvements are zoned as residential on the City General Plan Zoning map. Some of the areas are zoned as commercial or limited office area. The parcel along Fairgrounds Drive that is within the County Fairgrounds is zoned as public facilities.

IV. Dedications

Are there any property rights which have been acquired, or anticipated will be acquired, through the "dedication" process for the Project?

No X Yes _____ (Complete the following.)

Number of dedicated parcel _____

Have the dedicated parcels(s) been accepted by the municipality involved?

V. Excess Lands/Relinquishments

Are there Caltrans property rights which may become excess lands or potential relinquishment areas?

No X Yes _____ (Provide an explanation on additional page.)

RIGHT OF WAY DATA SHEET FOR LOCAL PUBLIC AGENCIES (Cont.)**Minimum Project Alternative** (Form #)R/W Data Sheet – Local Public Agencies
Page 3 of 5**VI. Relocation Information**

Are relocation displacements anticipated?

No _____ Yes X (Complete the following.)

A.	Number of Single Family Residential Units	<u> 13 </u>	
	Estimated RAP Payments		\$ <u>650,000</u>
B.	Number of Multifamily Residential Units	<u>1 Unit w/4 Relocations</u>	
	Estimated RAP Payments		\$ <u>100,000</u>
C.	Number of Business/Nonprofit	<u> 6 </u>	
	Estimated RAP Payments		\$ <u>240,000</u>
D.	Number of Farms	<u> </u>	
	Estimated RAP Payments		\$ <u> </u>
E.	Other (define in the "Remarks" section)	<u> </u>	
	Estimated RAP Payments		\$ <u> </u>
Totals		<u> 20 </u>	\$ <u>990,000</u>

VII. Utility Relocation Information

Do you anticipate any utility facilities or utility rights of way to be affected?

No _____ Yes X (Complete the following.)

Facility	Owner	Estimated Relocation Expense		
		State Obligation	Local Obligation	Utility Owner Obligation
A. Electrical	PG&E Distribution	\$	\$240,000	\$240,000
B. Gas	PG&E Distribution	\$	\$128,000	\$128,000
C. Water	City of Vallejo	\$	\$78,000	\$0
D.		\$	\$	\$
E.		\$	\$	\$
F.		\$	\$	\$
Totals		\$ *	\$446,000	\$368,000
Number of facilities			3	

*This amount reflects the estimated total financial obligation by the State.

Any additional information concerning utility involvement on this project? There are a total of 10 separate relocations required among the three facilities listed above.

RIGHT OF WAY DATA SHEET FOR LOCAL PUBLIC AGENCIES (Cont.)**Minimum Project Alternative** (Form #)R/W Data Sheet – Local Public Agencies
Page 4 of 5**VIII. Railroad Information**

Are railroad facilities or railroad rights of way affected?

No X Yes _____ (Complete the following.)

Describe railroad facilities or railroad rights of way affected.

Owner's Name	Transverse Crossing	Longitudinal Encroachment
A.		
B.		

Discuss types of agreements and rights required from the railroads. Are grade crossings that require services contracts, or grade separations that require construction and maintenance agreements involved?

IX. Clearance Information

Are there improvements that require clearance?

No _____ Yes X (Complete the following.)

A. Number of structures to be Demolished 20

Estimated Cost of Demolition \$ 1,257,000

X. Hazardous Materials/WasteAre there any site(s) and/or improvement(s) in the Project Limits that are known to contain *hazardous material*? None _____ Yes X (Explain in the "Remarks" section.)Are there any site(s) and/or improvement(s) in the Project Limits that are suspected to contain *hazardous material*? None _____ Yes X (Explain in the "Remarks" section.)**XI. Project Scheduling**

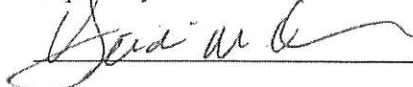
	Proposed lead time	Completion Date
*Preliminary Engineering, Surveys	<u>27</u> (months)	<u>6/15</u>
*R/W Engineering Submittals	<u>12</u> (months)	<u>6/16</u>
*R/W Appraisals/Acquisitions	<u>18</u> (months)	<u>12/16</u>
Proposed Environmental Clearance		<u>6/15</u>
Proposed R/W Certification		<u>5/17</u>

RIGHT OF WAY DATA SHEET FOR LOCAL PUBLIC AGENCIES (Cont.)**Minimum Project Alternative** (Form #)R/W Data Sheet – Local Public Agencies
Page 5 of 5**XII. Proposed Funding**

	Local	State	Federal	Other
Acquisition	\$11,214,000			
Utilities	\$814,000			
Relocation Assistance Program	\$990,000			
Clearance and Demolition	\$1,257,000			
Cost (R/W Support, Eng. Appraisals, etc.)	Included in PS&E Cost			
TOTAL	\$14,275,000			

XII. Remarks

Project construction areas consist of existing or previous locations of gasoline service stations, automobile service business. The Initial Site Assessment has identified hazardous materials within these parcels. One parcel containing a gas station was also not available for subsurface exploration and will require testing prior to acquisition.

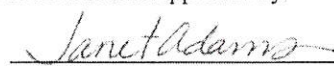
Project Sponsor Consultant
Prepared by:


Heidi Ouren – HQE Incorporated

5/19/15
Date

Project Sponsor

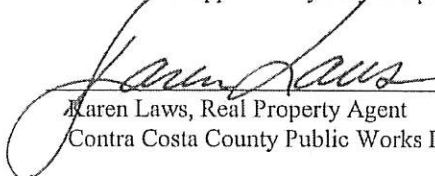
Reviewed and Approved by:



Janet Adams – Solano Transportation Authority

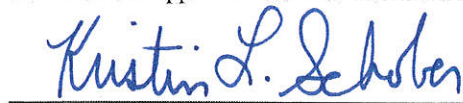
Date

Reviewed and approved by Project Sponsor Right of Way Specialist:


Karen Laws, Real Property Agent
Contra Costa County Public Works Department
5/19/15
Date

Caltrans

Reviewed and approved based on information provided to date:


Caltrans District Branch Chief
Local Programs
Division of Right of Way
6/4/15
Date

Attachment L
FHWA Engineering And
Operational Acceptability
Determination Letter



U.S. Department
of Transportation
**Federal Highway
Administration**

**Federal Highway Administration
California Division**

August 15, 2013

650 Capitol Mall, Suite 4-100
Sacramento CA 95814
(916) 498-5001
(916) 498-5008 fax

In Reply Refer To:
HDA-CA

File #: 04-SOL-80/04-SOL-37
PM 4.0/4.9 & 10.6/11.2
EA 04-4A4410

Mr. Bijan Sartipi, District Director
California Department of Transportation
District 4
P. O. Box 23660
Oakland, CA 94623-0660

Attention: Ms. Helena (Lenka) Culik-Caro, Deputy District Director, Design

Dear Mr. Sartipi:

This letter is in response to the California Department of Transportation (Caltrans)'s August 5, 2013, letter requesting a revised Engineering and Operational Acceptability (EOA) and approving a Fact Sheet Exceptions to Mandatory Design Standards for the proposed modification of access at the Interstate (I)-80/Redwood Parkway Interchange in the City of Vallejo, Solano County. The proposed project would improve existing interchanges, intersection operations, safety and relieve existing congestion on the local roadway network. The build alternative consists of modifying the I-80/Redwood Parkway interchange to a Type L-1 diamond interchange, widening Fairgrounds Drive from two to four lanes between Redwood Street and Coach Lane, widening Fairgrounds Drive from four to five lanes between Coach Lane and the State Route (SR) 37 eastbound entrance ramp, adding an exclusive right turn lane to the SR 37 westbound exit ramp, and modifying the lane configurations on Fairgrounds Drive at its intersections with SR 37.

Engineering and Operational Acceptability:

On July 18, 2012, FHWA issued the I-80/Redwood Parkway Interchange EOA for a 'Type L-9 Diamond Interchange', which was a typographical error in the Draft Project Report (DPR) submitted. The report had inadvertently specified the Type 9 interchange whereas the operational analysis was based on a Type I interchange configuration. This misprint was emulated in our July 18, 2012 EOA concurrence letter and the discrepancy was not identified until a request for a design exception approval was submitted to FHWA on July 17, 2013. Therefore, Caltrans is now requesting a revised EOA concurring in the "Type L-1 Diamond Interchange".

FHWA has concluded a re-evaluation of the Traffic Operations Report (TOAR) [April 2, 2012] and the unsigned Draft Project Report (DPR) [May 2013] to ensure consistency between these documents supporting a Type I Interchange configuration. Based on an engineering and operational analysis and under the authority of Section 111 of Title 23 U.S. Code, the build alternative, Type L-1, as documented in the TOAR is acceptable. This EOA approval only constitutes preliminary or conditional approval of the modified access request. If the Type I interchange alternative is ultimately selected as the preferred alternative in the environmental process, and there are no major changes in the proposed design, "final

approval” may be given upon completion of the environmental process by Caltrans. At that time, FHWA shall issue a formal letter of approval on the selected alternative. This EOA supersedes the July 18, 2012, EOA letter.

Mandatory Design Exception:

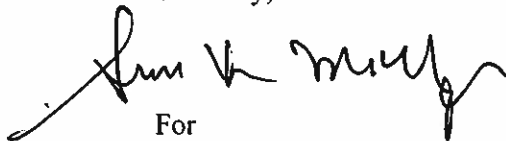
We also have reviewed the Fact Sheet Exception to Mandatory Design Standards (Fact Sheet) dated May 7, 2012, for the project. The Fact Sheet was sent to our office on 7/17/2013 via e-mail. The Fact Sheet describes and requests the following exceptions to the mandatory design standards:

1. Existing stopping sight distance is less than Caltrans Highway Design Manual (HDM)’s requirement at the locations identified on Page 5 of the Fact Sheet.
2. Proposed superelevation rate is less than the HDM’s requirement at the locations identified on Page 8 of the Fact Sheet.
3. Proposed corner sight distance on westbound exit ramp at the Redwood Street interchange is less than the HDM’s requirement.
4. Existing interchange spacing is less than the HDM’s requirement at the locations identified on Page 12 of the Fact Sheet.
5. Proposed distance between I-80 westbound ramp termini and Fairgrounds Drive intersection is less than the HDM’s requirement.
6. Proposed travelled way width of two left turn lanes onto the I-80/Redwood Street overcrossing structure is reduced to 11 feet, which is less than the HDM’s requirement.
7. Existing cross slopes on the Redwood Street overcrossing is less than the HDM’s requirement.
8. Existing vertical clearance (16 feet) between Redwood Street overcrossing and the I-80 westbound edge of shoulder is less than the HDM’s requirement.

Based on the supporting information and justification provided in the Fact Sheets, your request for the above noted exception to the mandatory design standard is approved.

If you have any questions, please contact Lanh Phan, Senior Transportation Engineer, at (916) 498-5046 or email at lanh.phan@dot.gov.

Sincerely,

A handwritten signature in black ink, appearing to read "Vin P. Mammano", written over a horizontal line.

For
Vincent P. Mammano
Division Administrator

cc: (via e-mail)

Ziad Abubekr, Caltrans

Stewart Lee, Caltrans

Marcus Chan, Caltrans

Jason Mac, Caltrans

Gary Sweeten, Caltrans

Peter Pangilinan, Caltrans

Jeff Holm, FHWA

Lanh Phan, FHWA

Lphan/DF

Attachment M

Risk Register

Project Risk Register

DIST- EA 04-4A4410						Project Name: Redwood Parkway/Fairground Drive Improvements Project			Project Manager: Jason Mac							Date Created:	Last Updated:
						Co - Rte - PM: Sol-37-10.6/11.2; Sol-80-4.0/4.9			Telephone: 510-622-8891								
ITEM	ID #	Status	Threat / Opport-unity	Category	Date Risk Identified	Risk Discription	Root Causes	Primary Objective	Overall Risk Rating	Cost/Time Impact Value	Risk Owner	Risk Trigger	Strategy	Response Actions w/ Pros & Cons	Adjusted Cost/Time Impact Value	WBS Item	Status Date and Review Comments
	(a)	(b)	(c)	(d)	(e)	(f)	(g)	(h)	(i)	(j)	(k)	(l)	(m)	(n)	(o)	(p)	(q)
1	04-4A4410-01	Active	Threat	ENV	10/15/08	Per the PSI, Class of ADL excavation material requires special handling by the contractor to dispose of excavated ADL material to be encapsulated within the State R/W.	Requirement	COST	Low		STA-Sol County-Vallejo	Contract No. 04-0T2404 required CCO's due to ADL being other than Type Y-2 Excavation	MITIGATE	During preparation of the contract plans, include requirements for handling of ADL material in the SSP's.		230 PREPARE DRAFT PS&E	
									Probability 1=Very Low (1-9%)								
									Impact 1 =Very Low								
2	04-4A4410-02	Active	Threat	EXT	10/15/08	Local Community Objects to Proposed Project	Customer	COST	Med		City of Vallejo	Opposition expressed at first Public forum	MITIGATE	Take into account during PA/ED phase of project development		165 PERFORM ENVIRONMENTAL STUDIES AND PREPARE DRAFT ENVIRONMENTAL DOCUMENT	
									Probability 3=Med (20-39%)								
									Impact 4 =Med								
3	04-4A4410-03	Retired	Threat	EXT	10/15/08	Project is required to accommodate future HOT Lanes.	Regulatory	COST	Low		STA-Sol County-Vallejo	MTC's Transporation 2035 adopted	ACCEPT	Take into account during PA/ED phase of project development		165 PERFORM ENVIRONMENTAL STUDIES AND PREPARE DRAFT ENVIRONMENTAL DOCUMENT	Risk has been retired. STA preparing a separate study for HOT Lanes. This project designed to accommodate furture HOT Lanes.
									Probability 1=Very Low (1-9%)								
									Impact 1 =Very Low								
4	04-4A4410-04	Active	Threat	EXT	10/15/08	Unreasonabe expectations from Stakeholders	Customer	COST	Med		STA-Sol County-Vallejo		TRANSFER	Transfer decision to higher level		165 PERFORM ENVIRONMENTAL STUDIES AND PREPARE DRAFT ENVIRONMENTAL DOCUMENT	Project team has coordinated with STA, County, and City staff to determine that the project is consistent with stakeholder's expectations.
									Probability 2=Low (10-19%)								
									Impact 4 =Med								
5	04-4A4410-05	Retired	Threat	DESIGN	10/15/08	Nonstandard Mandatory Features not Approved	Performance and Reliability	COST	Med		STA-Sol County-Vallejo	Design Coordinator denies request for Mandatory Design Exceptions	TRANSFER	Transfer decision to higher level		165 PERFORM ENVIRONMENTAL STUDIES AND PREPARE DRAFT ENVIRONMENTAL DOCUMENT	This is no longer an active risk. Mandatory and Advisory Exceptions have been approved.
									Probability 1=Very Low (1-9%)								
									Impact 8 =High								
6	04-4A4410-06	Active	Threat	ENV	03/01/11	Property not accessible to perform perliminary site investigation.	Requirement	COST	Med		STA-Sol County-Vallejo	Property is needed for improvements.	ACCEPT	During acquisition process, test soils.		185 PREPARE BASE MAPS AND PLAN SHEETS	
									Probability 2=Low (10-19%)								
									Impact 4 =Med								
7	04-4A4410-07	Active	Threat	ENV	08/24/12	USACOE requests consultation with USFWS under Section 7	Complexity and Interface	TIME	Low		STA-Sol County-Vallejo		ACCEPT	Request Env Permits early in PS&E stage		260 CONTRACT BID DOCUMENTS READY TO LIST	
									Probability 2=Low (10-19%)								
									Impact 2 =Low								
8	04-4A4410-08	Active	Threat	CON	04/15/11	Buried man-made objects encountered during construction	Complexity and Interface	TIME	Low		STA-Sol County-Vallejo	Native American site nearby. Excavations performed and nothing found.	ACCEPT			270 CONSTRUCTION ENGINEERING AND GENERAL CONTRACT ADMINISTRATION	
									Probability 1=Very Low (1-9%)								
									Impact 1 =Very Low								

Approved by:  3/10/15
date

Attachment N

Pavement Strategy

Review Checklist

PAVEMENT STRATEGY CHECKLIST

Date: September 10, 2012

Project description and project elements:

Modify ramps and ramp intersections at the I-80/Redwood Street interchange. Modify ramp
Termini of the SR37/Fairgrounds Drive EB on ramp and WB off ramp. Widen Fairgrounds
Drive between Redwood Street and the SR37/Fairgrounds Drive interchange.

EA: 4A4410Project Manager: Jason MacCo/Rte: Sol/37 & 80

Office: _____

Project Engineer: Garrett LowInitial GLProgram: 20.20.400.XXXDesign Senior: Stewart LeeInitial SLPM Limits: 37 PM 10.6/11.2, 80 PM 4.0/4.9Materials Engineer (8th floor) : Wei Yeh Lin Signature

This project is at the following phase (please check one):

☐ PID (PSSR, etc.) ☒ PR ☐ PS&E ☐ OTHER

Describe existing structural section (e.g., shoulder, traveled way). Show limits if different sections are within the project:

"C6" Line (Rte 80) - 0.70' OGAC/0.15' RAC (G)/0.65' AC(A)/0.50' AB(2)/0.67' PCC

"RE1" thru "RE4" Lines (Redwood St/Prkwy Ramps) - 0.50' AC(B)/0.20' ATPB/0.55' AB(2)/0.95' AS(4)

"FA4" Line (Fairground Dr. WB Off-Ramp) - 0.45' AC(B)/0.25' ATPB/0.60' AB(2)/1.20' PM(1)

"FAI" Line (Fairground Dr.) - 0.40' AC(B)/0.60' AB(2)/1.00' PM(1)

"RED" Line (Redwood St/Prkwy) - 0.40' AC(B)/0.80' AB(2)/1.30' AS(4)

What pavement types/structural sections does Materials propose for each segment (shoulders and traveled way)?

A. Assumed matching existing pavement section. Pavement design to be done during PS&E

B.

C.

Pavement is involved in:

☐ Entire project OR ☒ Part of the project

	Yes	No	Question
1.	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<p>Are you implementing an innovative strategy (e.g., cold foam Hot-Mix Asphalt (HMA)), pre-cast concrete pavement, continuously reinforced pavement, etc)?</p> <p>If so, which are you implementing and why? If not, why not? Structural section recommendations and design will be performed during final design. The majority of the new structural sections are on Redwood Street and Fairgrounds Drive which is under the jurisdiction of the City of Vallejo. For cost estimating purposes, the existing structural section was assumed for areas of new pavement/pavement widening.</p>
2.	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<p>Has Rapid Rehab strategy been considered (e.g., weekend closures and lane replacements)?</p> <p>Explain: No complete freeway closures or long term lane closures are needed for this project.</p>
3.	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<p>Are you using Rubberized Hot-Mix Asphalt (RHMA) in this project?</p> <p>If not, justify: will match existing pavement for ramp widening and realignments.</p>
4.	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<p>Was Life Cycle Analysis performed?</p> <p>Provide Life Cycle Analysis and results.</p>
5.	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<p>Does existing pavement have a settlement problem?</p> <p>Explain:</p>
6.	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<p>a) Is this project (or part of project) maintaining the grade profile?</p> <p>b) If not, explain how the profile change affects the pavement strategy choice (cut v. fill):</p>
7.	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Will there be a new barrier?
8.	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<p>Is the proposed structural section on cut or fill or both? Provide limits of both, if applicable.</p> <p>WB off ramp - "RE1" 233+78 to 234+90 & 236+50 to 237+15 (fill) WB on ramp - "RE2" 226+39 to 230+00 (fill) EB off ramp - "RE3" 225+20 to 232+85 (part on cut) EB on ramp - "RE4" 233+28 to 239+85 (fill)</p>
9.	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Are highly expansive basement soils present?

	Yes	No	Question
10.	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<p>Are as-builts (including structural section information regarding edge drains, under drains, lime treatment, permeable blanket, etc.) available? As-builts showing the existing structural sections were obtained.</p> <p><input type="checkbox"/> <input type="checkbox"/></p> <p>If no, did you check map files and online? N/A</p> <p>If yes, existing structural section was based on (check one): <input checked="" type="checkbox"/> as-built <input type="checkbox"/> actual boring</p>
11.	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<p>Do the project limits have problems with groundwater (e.g., high water table, flow requirements, etc.)? If yes, explain:</p>
12.	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<p>Has the availability of pavement materials (i.e., long haul distances from plants) been considered? Will be analyzed during final PS&E.</p> <p>If yes, how does material availability affect pavement type selection?</p>
13.	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<p>Will the existing pavement be rehabilitated?</p> <p><input type="checkbox"/> <input type="checkbox"/></p> <p>What are the age and condition of the existing adjacent lanes? Explain: A overlay project was completed in 2000 on the I-80 mainline. a new barrier was also constructed with this project. The existing pavement appears to be in good condition.</p>
14.	<input type="checkbox"/>	<input type="checkbox"/>	<p>What is the type of pavement/structural section (corridor pavement type/structural section continuity) on upstream/downstream roadway? Explain if several: Existing pavement through the corridor generally consists of: 0.16' OGAC, 0.15' RAC-G, 0.18' AC, 0.55' PCC</p>
15.	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<p>Is TMP data (lane closure charts) available and was it considered? Preliminary TMP has been prepared. No lane closure charts have been developed at this phase.</p> <p><input type="checkbox"/> <input checked="" type="checkbox"/></p> <p>Will there be nighttime paving? If so, provide lane closure hours: <u>TBD</u>.</p>
16.	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<p>Was field Maintenance input considered?</p>
17.	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<p>Were climate conditions (extreme temperature, rainfall, etc.) considered? This region does not experience extreme climatic conditions or excessive rainfall.</p> <p>If so, which ones do you anticipate affecting the pavement job?</p>

	Yes	No	Question
18.			Which stage construction requirements (matching adjacent sections, temporary paving, etc.) were considered? Matching existing and adjacent sections were considered.
19.	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Is this a large-scale project? Explain all quantity take-off: Quantity take offs were performed from the preliminary plans using actual designed dimensions.
20.	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Is there Open-Graded Hot-Mix Asphalt (OGHMA) on the existing pavement?
21.	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Was environmental impact considered? Explain: It was assumed that the new structural sections would match the existing and were used for cost estimating purposes. They matched the existing sections. Structural section recommendations and design will be developed during final design.
22.			What is the proposed pavement design life? Pavement design to be performed during final design. The design life will be determined by those shown in Table 612.2 of the Highway Design Manual (minimum of 20 years).
23.			What is the final lane line configuration? See Layout plans and typical cross sections.
24.	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Are there vertical clearance issues? If yes, explain:
25.			What is the traffic index? To be determined during pavement design during final design.
26.	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Are there existing retrofit edge drains?
27.	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Will shoulders be used as detours?
28.	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Is there settlement at bridge approaches?
	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Are bridge approach slabs being replaced? Does such replacement include shoulders?
			Consulted with structures maintenance representative on _____.
29.	<input checked="" type="checkbox"/>	<input type="checkbox"/>	Is there a minimum standard (2% or 1.5%) cross-slope? If not standard, provide date of design exception approval: <u>5/7/12</u>
30.			Provide the pavement condition report.

	Yes	No	Question
31	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Other factors? Explain: